



NOTICE OF APPROVAL OF A HOUSING SUSTAINABILITY DISTRICT PROJECT

Record No.: 2021-000947PRJ
Project Address: 555-585 Bryant Street
Zoning: Central Soma Mixed Use Office (CMUO) Zoning District
Central SoMa Special Use District (SUD)
130-CS Height and Bulk District
Block/Lot: 3776 / 34, 38, 39, 40, 42, 43, 44
Project Sponsor: John Kevlin
Reuben, Junius & Rose, LLP
One Bush Street, Suite 600
San Francisco, CA 94104
jkevin@reubenlaw.com
Property Owner: 555 Bryant: Bryant Partners, LLC; 565-585 Bryant: Bryant & Welsh LLC
Staff Contact: Xinyu Liang – (628) 652-7316
Xinyu.Liang@sfgov.org

Project Description

The Project proposes to combine seven lots between Bryant and Welsh Streets and replace the existing PDR buildings with a 160-foot-tall mixed-use multi-family residential building with 500 dwelling units, 20,605 square feet of PDR use space, 125 accessory parking spaces, and 200 Class One and 32 Class Two bicycle parking spaces.

The Project's façade will incorporate two mass reduction breaks for more appropriate building articulation. Angled bay windows provide a sense of visual movement with less solidity as the building rises. Building frontages are proposed with active ground floor uses fronting Bryant Street and townhome-style residential walkup units fronting Welsh Street. The material palette of the Project will fit into the surrounding neighborhood and features large windows, metal-clad bay windows, as well as wood and masonry panels for some visual interests on the pedestrian level. The Project will also include a series of streetscape improvements, including sidewalk widening on both Bryant and Welsh Streets and new street trees.

Background

In December 2018, Ordinance No. 281-18 was adopted, amending the Business and Regulations and Planning Codes to create the Central South of Market Housing Sustainability District (HSD) under Planning Code Section 343. The HSD designates the Planning Department as the approving authority for Project eligible for the HSD and designates a ministerial approval process, wherein the Project is approved without discretion if it complies with

the requirements of Section 343, all other requirements of the Planning Code, and complies with all applicable mitigation measures of the Central SoMa EIR.

On February 12, 2021, John Kevlin from Reuben, Junius & Rose on behalf of Bryant Partners, LLC and Bryant & Welsh LLC (hereafter “Project Sponsor”) submitted Building Permit Application No. 202102124635, proposing to replace the existing production, distribution and repair (PDR) buildings with a 160-foot-tall mixed-use residential building with 500 dwelling units, 20,605 square feet of PDR use space, 125 accessory parking spaces, and 202 Class One and 27 Class Two bicycle parking spaces.

The project is seeking ministerial approval under the HSD. Additionally, the Project Sponsor submitted an application for an Individually Requested State Density Bonus (CA Government Code Section 65915 *et seq.*) for a 42% increase in residential floor area, including Waivers from the Setback and Street Wall (Planning Code Section (Sec.) 132.4), Permitted Obstruction for Bay Window (Sec. 136), Ground Floor Ceiling Height (Sec. 145.1 and 249.78), Residential Open Space (Sec. 135), Lot Coverage (Sec. 249.78), Wind Comfort (Sec. 249.78), Height Limit (Sec. 260), Narrow Street and Alley (Sec. 261.1), Apparent Mass Reduction (Sec. 270), Horizontal Mass Reduction (Sec. 270.1), and Mid-block Alley in Large Lots (Sec. 270.2) controls of the Planning Code, as well as Incentives / Concessions from the Central SoMa SUD Living Roof Standards (Sec. 149 and 247.78) and Curb Cut on Transit Preferential Street (Sec. 155) requirements of the Planning Code.

HSD Approval

The Project is seeking approval through the ministerial approval process provided by the HSD. To be eligible for approval under the HSD, a Project must meet the following conditions:

1. The project is located in a zoning district that principally permits residential uses.

Residential Dwelling Units are principally permitted in the Central SoMa Mixed-Use Office Zoning District.

2. The project proposes no less than 50 dwelling units per acre, and no more than 750 dwelling units per acre.

The project proposes 500 Dwelling Units on a 0.7529-acre site, for a total density of 664 units per acre.

3. A majority of the project’s gross square footage is designated for residential uses. All non-residential uses must be principally permitted in the underlying zoning district and any applicable special use district(s), and may not include greater than 24,999 gross square feet of office space that would be subject to the annual limit on office development set forth in Sections [321](#) *et seq.*

The majority of the square footage of the proposed project is devoted to residential uses, and the proposed ground floor PDR space is principally permitted. The project includes no office use.

4. The project does not exceed a height of 160 feet, except that any project whose principal use is housing, where all such housing is restricted for a minimum of 55 years as affordable for “persons and families of

low or moderate income,” as defined in California Health & Safety Code Section 50093, shall be deemed to satisfy this subsection (c)(4) regardless of height.

The proposed project is 160-feet in height and does not exceed this limit except for features exempt from height limits.

5. If the project sponsor seeks a density bonus pursuant to California Government Code Section 65915 *et seq.*, the project sponsor demonstrates to the satisfaction of the Planning Department that the project would not result in a significant shadow impact.

The shadow analysis of the Project establishes that the Project does not cause any significant shadow impact.

6. The project is not located on a lot containing a structure listed as a designated landmark pursuant to [Article 10](#) of the Planning Code or a contributory or significant structure pursuant to [Article 11](#) of the Planning Code.

The project site does not contain any structure listed as a designated landmark pursuant to Article 10 of the Planning Code or any contributory or significant structure pursuant to Article 11 of the Planning Code.

7. The project complies with the following affordability requirements, as applicable:

- A. Projects subject to Section [415](#) shall comply with Section [415](#) by choosing the On-Site Affordable Housing Alternative under Section [415.5\(g\)\(1\)\(A\)](#), and shall provide no less than 10% of dwelling units as units affordable to very low or low income families.

The project provides inclusionary housing units on-site, with at least 10% of dwelling units (or 50 dwelling units) as units affordable to very low or low income families.

- B. Projects not subject to Section [415](#) shall provide no less than 10% of dwelling units as units affordable to very low or low income families, by entering into a regulatory agreement with the City that contains the terms specified in Section [206.6\(f\)](#).

The project is subject to Planning Code Section 415 and shall provide inclusionary affordable housing units in compliance with Planning Code Section 415. The project satisfies the Inclusionary Affordable Housing Program (“Inclusionary Program”) through the Combination Alternative set forth in Section 415(g)(1)(D), and provides 21% of 352 base dwelling units as on-site affordable units. The State Density Bonus law offers a 42.5% density bonus in exchange for providing 13% of the base project to very-low income units. In order to qualify for this bonus, the project sponsor will provide four additional affordable- units at 50% AMI to qualify for the 42.5% density bonus; therefore the total number of affordable units is 78 (up from 74 units). Additionally, the project team is considering pursuing South Beach Marina affordable housing credits pursuant to Planning Code Section 202.8(f)(6) and Ordinance 197-16 in order to satisfy Inclusionary Program requirements. Should South Beach Marina affordable housing credits be pursued in the future, a subsequent Housing Sustainability District approval and revised Notice of Special Restrictions will follow.

8. The project does not demolish, remove, or convert to another use any existing dwelling unit(s).

The project site contains no existing housing.

9. The project complies with all applicable zoning and any adopted design review standards.

The project complies with all applicable zoning and any adopted design review standards.

10. The project sponsor complies with all Mitigation Measures in the Central SoMa Environmental Impact Report (Central SoMa EIR) that the Planning Department determines are applicable to the project.

The project is required to comply with all Mitigation Measures in the Central SoMa Environmental Impact Report (Central SoMa EIR) that are included in the Mitigation Monitoring and Reporting Program that the Planning Department determined applies to the project. The Project Sponsor must comply with the project's Mitigation Monitoring and Reporting Program on June 29, 2021. See Exhibit B.

11. The project sponsor certifies that the project will comply with all applicable requirements of California Government Code Section 66201(f)(4).

The Project Sponsor has certified that the project will comply with all applicable requirements of California Government Code Section 66201(f)(4).

12. The project shall comply with Government Code Section 66201(f)(5).

The Project Sponsor has certified that the project will comply with Government Code Section 66201(f)(5).

13. A project is not deemed to be for residential use if it is infeasible for actual use as a single or multifamily residence.

The Project Site is feasible for use as a multi-family residential development.

Concessions and Incentives

The Project has requested concessions/incentives from the development standards for a garage entry located on a Transit Preferential Street (Planning Code Section 155(r)) and the Central SoMa Special Use District Living Roof Standards (Planning Code Section 249.78(d)(4)). Pursuant to Planning Code Section 206.6, the Department shall grant the concessions or incentives requested by the Applicant unless the Department makes a written finding, based upon substantial evidence, of any of the following:

- A. The concession or incentive does not result in identifiable and actual cost reductions, consistent with subdivision (k), to provide for affordable housing costs, as defined in Section 50052.5 of the Health and Safety Code, or for rents for the targeted units to be set as specified in subdivision (c).

Transit Preferential Street. Planning Code Section 155(r) requires that no curb cuts accessing off-street parking or loading shall be created or expanded on street frontages identified along any Transit Preferential Street as designated in the Transportation Element of the General Plan.

The Project proposes to locate a garage entrance on Bryant Street, a Transit Preferential Street between 2nd and 6th Streets, in order to provide more efficient ingress and egress, which reduces the cost of off-site work and non-residential space in the building, and allows the Project to provide more housing. The only other option for a garage entrance would be on Welsh Street, in place of ground-floor residential units. The Welsh Street alternative would require significant curb ramp realignments on surrounding streets where there are conflicts that prevent adequate truck turning radii. These conflicts do not exist for the proposed Bryant Street entrance location. The Welsh Street alternative would also result in more non-residential space and fewer units, thus increasing the cost per unit to deliver market-rate and affordable housing.

Central SoMa Living Roof Requirement. *The Project is required to provide a living roof area equal to 50% of the roof area, or 12,353 square feet. The Project will provide a total of 8,235 square feet of living roof area, equal to 33% of the roof area. The provision of additional living roof area would cause the amount of usable open space to be decreased, causing an in-lieu fee of \$971.81 per square foot for the open space not provided. Additionally, the Project Sponsor states that constructing an additional living roof would increase the cost of constructing the roof surface and structural support. As such, a Concession from the Central SoMa living roof requirements of the Planning Code decreases the cost of constructing the Project.*

- B. The concession or incentive would have a specific, adverse impact, as defined in paragraph upon public health and safety or the physical environment or on any real property that is listed in the California Register of Historical Resources and for which there is no feasible method to satisfactorily mitigate or avoid the specific, adverse impact without rendering the development unaffordable to low-income and moderate-income households.

The requested concessions/incentives from the development standards for a garage entry on a Transit Preferential Street and Central SoMa Living Roof would not result in a specific, adverse impact to public health and safety or the physical environment, or on any real property that is listed in the California Register of Historical Resources.

- C. The concession or incentive would be contrary to state or federal law.

The Project's requested concessions/incentives from the development standards would not be contrary to state or federal law.

Waivers

In no case may the Department apply any development standard that will have the effect of physically precluding the construction of a development at the densities or with the concessions or incentives permitted by the State Density Bonus Law. The Department is not required to waive or reduce development standards if the waiver or reduction would have a specific, adverse impact upon health, safety, or the physical environment, and for which there is no feasible method to satisfactorily mitigate or avoid the specific adverse impact. The Department is not required to waive or reduce development standards that would have an adverse impact on any real property that is listed in the California Register of Historical Resources, or to grant any waiver or reduction that would be contrary to state or federal law.

The requested waivers from the setback and streetwall articulation, permitted obstructions, ground floor ceiling height, residential open space, lot coverage, wind comfort levels, height limit, narrow street and alley setbacks, apparent mass reduction, horizontal mass reduction, and mid-block alley requirements are required to accommodate the Project with the requested concessions or incentives permitted by the Density Bonus Law.

Setback and Streetwall Articulation. *Planning Code Section 132.4 requires that Mid-Rise Buildings must provide a 15-foot setback for 60% of the street- and alley-facing property lines and all interior property lines at a height of 85 feet and above. The Project provides no setback along property lines and requires a waiver.*

Residential Open Space. *Per Planning Code Section 135, the minimum residential open space requirement is 80 square feet per dwelling unit. The Project proposes 500 dwelling units and therefore 40,000 square feet of residential open space is required. The Project will provide approximately 8,235 square feet of code-compliant open space on the roof and therefore requires a waiver.*

Permitted Obstructions. *Per Planning Code Section 136, bay windows may encroach beyond a street- or alley-facing property line so long as the bay window meets the certain size and pattern limitations. The Project proposes a bay window design that does not comply with the glazing on two sides and horizontal dimension requirements and therefore requires a waiver.*

Ground Floor Ceiling Height. *Per Planning Code Section 145.1, the minimum height limit for ground floors in the CMOU zoning district is 17 feet with ground floor PDR use or 14 feet without PDR use. The Project proposes a ground floor height of 13.33 feet and therefore requests a waiver.*

Lot Coverage. *Per Planning Code Section 249.78(d)(6), the Central SoMa SUD sets a lot coverage limit of 80% unless all dwelling units face onto a public street. The proposed project does not comply and exceeds the maximum by providing 86 % lot coverage; thus, a waiver for lot coverage is required to accommodate the residential habitable floor area.*

Wind Comfort and Wind Hazard Levels. *Per Planning Code Section 249.78(d)(9), the Central SoMa SUD establishes that a project may not result in wind speeds that exceed the Comfort Level nor causes a Substantial Increase in wind speeds at any location. The proposed project causes a Substantial Increase in wind speeds at two locations. The Project has incorporated landscaping into its design that will eliminate both Substantial Increase locations. Also, the Central SoMa SUD establishes that a project may not result in wind speeds that exceed the One-Hour Hazard Criterion at any location. The Project causes an exceedance of the One-Hour Hazard Criterion at two locations. When landscaping is incorporated into the Project, it eliminates one of those exceedances. The remaining exceedance is well below the Nine-Hour Hazard Criterion established by the Central SoMa SUD and has been determined to have no significant impact under CEQA.*

The Project Sponsor has also conducted a series of testing to evaluate the impact of extensive alternative wind control measures on the proposed development in combination with landscaping. The various canopies and architectural fins for wind control on the proposed development were found to have a

relatively negligible impact on the average wind comfort and hazard wind speeds across all test locations. Reducing wind speeds further would substantially detract from the building design or unduly restrict the square footage of the project; thus, the Project is seeking a waiver for Wind Comfort and Wind Hazard Controls.

Height. *Per Planning Code Section 250, the maximum height limit for the subject property is 130 feet. The Project proposes a height of 160 feet to accommodate the additional floor area. 160 feet is the maximum height allowed in the Housing Sustainability District. Exceeding the underlying 130-foot height and bulk district requires a height waiver.*

Narrow Street and Alley Setbacks. *The Planning Code Section 261.1 and 270(h) require that the Project's massing along Welsh Street comply with the Apparent Mass Reduction requirement of Section 270(h) for a reduced massing at 85% above 35 feet in height. The proposed project only provides 1.95% of reduction, which does not comply with the Apparent Mass Reduction requirement and thus a waiver is necessary.*

Apparent Mass Reduction. *The Apparent Mass Reduction requirement of Planning Code Section 270(h) requires a reduced massing at 67% above 85 feet in height for a Mid-Rise Building. The proposed project only provides 1.95% of reduction, and thus a waiver is necessary.*

Horizontal Mass Reduction. *The Planning Code Section 270.1 requires certain mass reduction measures to be incorporated into a project that has a street frontage of more than 200 linear feet. The proposed project does not provide this mass reduction and thus a waiver is necessary.*

Mid-Block Alley. *The Planning Code Section 270.2 requires a Mid-Block Alley be provided on project sites with more than 200 linear feet of frontage where such alley would connect to existing streets or alleys. The proposed project does not provide a mid-block alley and thus a waiver is necessary.*

The Project is not seeking any waivers that would have an adverse impact to the real property listed on the California Register of Historical Resources, and is not seeking any waiver that is contrary to state or federal law.

Approval

The Department has determined that the Project meets all the objective standards of the Planning Code and has completed design review of the project.

The Project has been approved in accordance with the provisions of the Central SoMa Housing Sustainability District, as recorded in Building Permit Application No. 202102124635.

Therefore, as the Project complies with the requirements of the Central SoMa Housing Sustainability District (Planning Code Section 343), the Project is approved pursuant to the Notice of Special Restrictions attached as Exhibit A, the Mitigation Monitoring and Reporting Program requirements attached as Exhibit B, and the Approved Plans attached as Exhibit C. The Department shall review and approve any modification to the approved plans, including the application of the South Beach Marina Credits, prior to permit issuance.



Signature: _____

Rich Hillis, Planning Director

Date: 06/30/2021

Attachments:

- Exhibit A: Conditions of Approval (Notice of Special Restrictions)
- Exhibit B: Mitigation Monitoring and Reporting Program
- Exhibit C: Approved Plans
- Exhibit D: California Department of Housing and Community Development (HCD) certification of the Central SoMa Housing Sustainability District

NOTICE OF SPECIAL RESTRICTIONS UNDER THE PLANNING CODE

RECORDING REQUESTED BY:)
)
And When Recorded Mail To:)
)
Name:)
)
Address:)
)
City:)
)
State: California Zip:) Space Above this Line For Recorder's Use

I, (We) _____, the owner(s) of that certain real property situated in the City and County of San Francisco, State of California more particularly on attached sheet marked "Exhibit A,

BEING ASSESSOR'S BLOCK: 3776, LOT(S): 34, 38, 39, 40, 42, 43, 44,

COMMONLY KNOWN AS: 555-585 Bryant Street,

hereby give notice that there are special restrictions on the use of said property under Part II, Chapter II of the San Francisco Municipal Code (Planning Code).

Said restrictions consist of conditions attached to Building Permit Application No. **202102124635** by the Planning Department and are conditions that had to be so attached in order that said application could be approved under the Planning Code (Planning Case No. 2021-000947PRJ), Housing Sustainability District (CA Govt. Code Sections 66200 et seq) and the State Density Bonus Law (CA Govt. Code Section 65915). The plans filed with the present application indicate that the proposed project at 555-585 Bryant Street include the construction of a 160-foot-tall mixed-use multi-family residential building with 500 dwelling units, 20,605 square feet of PDR use space (the "Project").

Recordation of Conditions of Approval

Prior to the issuance of the building permit or commencement of use for the Project the Zoning Administrator shall approve and order the recordation of a Notice in the Official Records of the Recorder of the City and County of San Francisco for the subject property. This Notice shall state that the project is subject to the conditions of approval contained herein and reviewed and approved by the Planning Department on **June 30, 2021**.

Severability

The Project shall comply with all applicable City codes and requirements. If any clause, sentence, section, or any part of these conditions of approval is for any reason held to be invalid, such invalidity shall not affect or impair other remaining clauses, sentences, or sections of these conditions. This

NOTICE OF SPECIAL RESTRICTIONS UNDER THE PLANNING CODE

decision conveys no right to construct, or to receive a building permit. “Project Sponsor” shall include any subsequent owner or responsible party.

Changes and Modifications

Changes to the approved plans may be approved administratively by Planning Staff if the modifications to the Project are compliant with the objective provisions of the Planning Code.

Conditions of Approval, Compliance, Monitoring, and Reporting

Performance

- 1. Validity and Expiration.** The authorization and right vested by virtue of this action is valid for thirty (30) months from the effective date of this approval. The Department of Building Inspection shall have issued a Building Permit or Site Permit to construct the project and/or commence the approved use within this thirty-month period. If the Planning Director finds that the project sponsor has demonstrated good faith in its efforts to obtain a Building Permit or Site Permit within this thirty-month period, the Director may extend the approval for a maximum of six additional months. Such deadline shall additionally be extended in the event of any appeal of such approval for the duration of the appeal, and in the event of litigation seeking to invalidate the approval for the duration of the litigation.

For information about compliance, contact Code Enforcement, Planning Department at 628.652.7463, www.sfplanning.org

- 2. New Application.** Should a Building or Site Permit be sought after the period described in Condition 1 has lapsed, the project sponsor must seek a new Authorization by filing a new application for Authorization.

For information about compliance, contact Code Enforcement, Planning Department at 628.652.7463, www.sfplanning.org

- 3. Diligent Pursuit.** Once a site or Building Permit has been issued, construction must commence within the timeframe required by the Department of Building Inspection and be continued diligently to completion-

For information about compliance, contact Code Enforcement, Planning Department at 628.652.7463, www.sfplanning.org

- 4. Conformity with Current Law.** No application for Building Permit, Site Permit, or other entitlement shall be approved unless it complies with all applicable provisions of City Codes in effect at the time of such approval.

For information about compliance, contact Code Enforcement, Planning Department at 628.652.7463,

NOTICE OF SPECIAL RESTRICTIONS UNDER THE PLANNING CODE

www.sfplanning.org

- 5. Mitigation Measures.** Mitigation measures described in the MMRP attached as Exhibit B are necessary to avoid potential significant effects of the proposed project and have been agreed to by the project sponsor. Their implementation is a condition of project approval.

For information about compliance, contact Code Enforcement, Planning Department at 628.652.7463, www.sfplanning.org

Design – Compliance at Plan Stage

- 6. Final Materials.** The Project Sponsor shall continue to work with Planning Department on the building design. Final materials, glazing, color, texture, landscaping, and detailing shall be subject to Department staff review and approval. The architectural addenda shall be reviewed and approved by the Planning Department prior to issuance.

For information about compliance, contact the Case Planner, Planning Department at 628.652.7316, www.sfplanning.org

- 7. Garbage, Composting and Recycling Storage.** Space for the collection and storage of garbage, composting, and recycling shall be provided within enclosed areas on the property and clearly labeled and illustrated on the building permit plans. Space for the collection and storage of recyclable and compostable materials that meets the size, location, accessibility and other standards specified by the San Francisco Recycling Program shall be provided at the ground level of the buildings.

For information about compliance, contact the Case Planner, Planning Department at 628.652.7316, www.sfplanning.org

- 8. Overhead Wiring.** The Property owner will allow MUNI to install eyebolts in the building adjacent to its electric streetcar line to support its overhead wire system if requested by MUNI or MTA.

For information about compliance, contact San Francisco Municipal Railway (Muni), San Francisco Municipal Transit Agency (SFMTA), at 415.701.4500, www.sfmta.org

- 9. Noise, Ambient.** Interior occupiable spaces shall be insulated from ambient noise levels. Specifically, in areas identified by the Environmental Protection Element, Map1, “Background Noise Levels,” of the General Plan that exceed the thresholds of Article 29 in the Police Code, new developments shall install and maintain glazing rated to a level that insulate interior occupiable areas from Background Noise and comply with Title 24.

For information about compliance, contact the Environmental Health Section, Department of Public Health at 415.252.3800, www.sfdph.org

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- 10. Rooftop Mechanical Equipment.** Pursuant to Planning Code 141, the Project Sponsor shall submit a roof plan to the Planning Department prior to Planning approval of the building permit application. Rooftop mechanical equipment, if any is proposed as part of the Project, is required to be screened so as not to be visible from any point at or below the roof level of the subject building.

For information about compliance, contact the Case Planner, Planning Department at 628.652.7316, www.sf-planning.org

- 11. Lighting Plan.** The Project Sponsor shall submit an exterior lighting plan to the Planning Department prior to Planning Department approval of the building / site permit application.

For information about compliance, contact the Case Planner, Planning Department at 628.652.7316, www.sf-planning.org

- 12. Streetscape Plan.** Pursuant to Planning Code Section 138.1, the Project Sponsor shall continue to work with Planning Department staff, in consultation with other City agencies, to refine the design and programming of the Streetscape Plan so that the plan generally meets the standards of the Better Streets Plan and all applicable City standards. The Project Sponsor shall complete final design of all required street improvements and have permits for the required streetscape improvements on file with the City prior to issuance of the site permit, and shall complete construction of all required street improvements prior to issuance of first temporary certificate of occupancy.

For information about compliance, contact the Case Planner, Planning Department at 628.652.7316, www.sf-planning.org

- 13. Signage.** The Project Sponsor shall develop a signage program for the Project which shall be subject to review and approval by Planning Department staff before submitting any building permits for construction of the Project. All subsequent sign permits shall conform to the approved signage program. Once approved by the Department, the signage program/plan information shall be submitted and approved as part of the site permit for the Project. All exterior signage shall be designed to complement, not compete with, the existing architectural character and architectural features of the building.

For information about compliance, contact the Case Planner, Planning Department at 628.652.7316, www.sf-planning.org

- 14. Transformer Vault Location.** The location of individual project PG&E Transformer Vault installations has significant effects to San Francisco streetscapes when improperly located. However, they may not have any impact if they are installed in preferred locations. The Project Sponsor will continue to work with the Planning Department in consultation with Public Works on the final location(s) for transformer vaults. The above requirement shall adhere to the

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Memorandum of Understanding regarding Electrical Transformer Locations for Private Development Projects between Public Works and the Planning Department dated January 2, 2019.

For information about compliance, contact Bureau of Street Use and Mapping, Department of Public Works at 415-554-5810, <http://sfdpw.org>.

Parking and Traffic

15. Transportation Demand Management (TDM) Program. Pursuant to Planning Code Section 169, the Project shall finalize a TDM Plan prior to the issuance of the first Building Permit or Site Permit to construct the project and/or commence the approved uses. The Property Owner, and all successors, shall ensure ongoing compliance with the TDM Program for the life of the Project, which may include providing a TDM Coordinator, providing access to City staff for site inspections, submitting appropriate documentation, paying application fees associated with required monitoring and reporting, and other actions.

Prior to the issuance of the first Building Permit or Site Permit, the Zoning Administrator shall approve and order the recordation of a Notice in the Official Records of the Recorder of the City and County of San Francisco for the subject property to document compliance with the TDM Program. This Notice shall provide the finalized TDM Plan for the Project, including the relevant details associated with each TDM measure included in the Plan, as well as associated monitoring, reporting, and compliance requirements.

For information about compliance, contact the TDM Performance Manager at tdm@sfgov.org or 628.652.7340, www.sfplanning.org

16. Bicycle Parking. The Project shall provide no fewer than 200 Class 1 and 32 Class 2 bicycle parking spaces as proposed and consistent with Planning Code Sections 155.1 and 155.2.

For information about compliance, contact Code Enforcement, Planning Department at 628.652.7316, www.sfplanning.org

17. Off-Street Loading. Pursuant to Planning Code Section 152, the Project will provide one off-street loading space and two service vehicle spaces.

For information about compliance, contact Code Enforcement, Planning Department at 628.652.7316, www.sfplanning.org

18. Managing Traffic During Construction. The Project Sponsor and construction contractor(s) shall coordinate with the Traffic Engineering and Transit Divisions of the San Francisco Municipal Transportation Agency (SFMTA), the Police Department, the Fire Department, the Planning Department, and other construction contractor(s) for any concurrent nearby Projects to manage traffic congestion and pedestrian circulation effects during construction of the Project.

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For information about compliance, contact Code Enforcement, Planning Department at 628.652.7316, www.sfplanning.org

- 19. Driveway Loading and Operations Plan (DLOP).** Pursuant to Planning Code Section 155(u), the Project sponsor shall prepare a DLOP for review and approval by the Planning Department, in consultation with the San Francisco Municipal Transportation Agency. The DLOP shall be written in accordance with any guidelines issued by the Planning Department.

For information about compliance, contact Code Enforcement, Planning Department at 628.652.7316, www.sfplanning.org

Provisions

- 20. Prevailing Wage and Skilled Workforce Requirements.** If the Project is not in its entirety a public work, as defined in Government Code Section 65913.4 (a)(8)(A), all construction workers employed in the execution of the development must be paid at least the general prevailing rate of per diem wages for the type of work and geographic area. A project sponsor shall submit weekly reports to the Office of Labor Standards Enforcement, certifying that a project approved pursuant to Section 343 is complying with subsections (d)(11) and (d)(12), if applicable to the project. The project sponsor certifies that the project will comply with all applicable requirements of California Government Code Section 66201(f)(4). The project shall comply with Government Code Section 66201(f)(5). Projects found to be in violation of subsections (d)(11) and (d)(12) shall be subject to penalties pursuant to Section 1741 of the Labor Code, in addition to any penalties assessed pursuant to Section 176.1 of this Code. All penalties shall be paid prior to issuance of the project's First Certificate of Occupancy.

For information about compliance, contact the Case Planner, Planning Department at 628.652.7316, www.sf-planning.org

- 21. Anti-Discriminatory Housing.** The Project shall adhere to the requirements of the Anti-Discriminatory Housing policy, pursuant to Administrative Code Section 1.61.

For information about compliance, contact the Case Planner, Planning Department at 628.652.7316, www.sfplanning.org

- 22. First Source Hiring.** The Project shall adhere to the requirements of the First Source Hiring Construction and End-Use Employment Program approved by the First Source Hiring Administrator, pursuant to Section 83.4(m) of the Administrative Code. The Project Sponsor shall comply with the requirements of this Program regarding construction work and on-going employment required for the Project.

For information about compliance, contact the First Source Hiring Manager at 415.581.2335, www.onestopSF.org

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- 23. Transportation Sustainability Fee.** The Project is subject to the Transportation Sustainability Fee (TSF), as applicable, pursuant to Planning Code Section 411A.

For information about compliance, contact the Case Planner, Planning Department at 628.652.7316, www.sfplanning.org

- 24. Residential Child Care Impact Fee.** The Project is subject to the Residential Child Care Fee, as applicable, pursuant to Planning Code Section 414A.

For information about compliance, contact the Case Planner, Planning Department at 628.652.7316, www.sfplanning.org

- 25. Eastern Neighborhoods Infrastructure Impact Fee.** The Project is subject to the Eastern Neighborhoods Infrastructure Impact Fee, as applicable, pursuant to Planning Code Section 423.

For information about compliance, contact the Case Planner, Planning Department at 628.652.7316, www.sf-planning.org

- 26. Central SoMa SUD, Renewable Energy Requirements.** The Project shall fulfill all on-site electricity demands through any combination of on-site generation of 100% greenhouse gas-free sources in compliance with Planning Code Section 249.78(d)(5).

For information about compliance, contact the Case Planner, Planning Department at 628.652.7316, www.sf-planning.org

- 27. State Density Bonus Regulatory Agreement.** Recipients of development bonuses under Planning Code Section 206.3/206.6 shall enter into a Regulatory Agreement with the City, as follows.

- A. The terms of the agreement shall be acceptable in form and content to the Planning Director, the Director of MOHCD, and the City Attorney. The Planning Director shall have the authority to execute such agreements.
- B. Following execution of the agreement by all parties, the completed Regulatory Agreement, or memorandum thereof, shall be recorded and the conditions filed and recorded on the Housing Project.
- C. The approval and recordation of the Regulatory Agreement shall take place prior to the issuance of the First Construction Document. The Regulatory Agreement shall be binding to all future owners and successors in interest.
- D. The Regulatory Agreement shall be consistent with the guidelines of the City's Inclusionary Housing Program and shall include at a minimum the following:
 - i. The total number of dwelling units approved for the Housing Project, including the number of restricted affordable units;
 - ii. A description of the household income group to be accommodated by the restricted

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- affordable units, and the standards for determining the corresponding Affordable Rent or Affordable Sales Price. If required by the Procedures Manual, the project sponsor must commit to completing a market survey of the area before marketing restricted affordable units;
- iii. The location, dwelling unit sizes (in square feet), and number of bedrooms of the restricted affordable units;
 - iv. Term of use restrictions for the life of the project;
 - v. A schedule for completion and occupancy of restricted affordable units;
 - vi. A description of any Concession, Incentive, waiver, or modification, if any, being provided by the City;
 - vii. A description of remedies for breach of the agreement (the City may identify tenants or qualified purchasers as third party beneficiaries under the agreement); and
 - viii. Other provisions to ensure implementation and compliance with Section 206.3/206.6.

For information about compliance, contact the Case Planner, Planning Department at 628.652.7316, www.sf-planning.org or the Mayor's Office of Housing and Community Development at (415) 701-5500, www.sfmohcd.org.

Affordable Housing

- 28. Number of Required Units.** The Project complies with the State Density Bonus Program and the Inclusionary Affordable Housing requirements of Section 415 of the Planning Code by providing 74 on-site affordable units. The project satisfies the Inclusionary Affordable Housing Program ("Inclusionary Program") through the Combination Alternative set forth in Section 415(g)(1)(D), and provides 21% of 352 base dwelling units as on-site affordable units. The State Density Bonus law offers a 42.5% density bonus in exchange for providing 13% of the base project to very-low income units. In order to qualify for this bonus, the project sponsor will provide four additional affordable-units at 50%AMI to qualify for the 42.5% density bonus; therefore the total number of affordable units is 78 (up from 74 units). If the number of market-rate units change, the number of required affordable units shall be modified accordingly with written approval from Planning Department staff in consultation with the Mayor's Office of Housing and Community Development ("MOHCD").

For information about compliance, contact the Case Planner, Planning Department at (628) 652-7600, www.sfplanning.org or the Mayor's Office of Housing and Community Development at (415) 701-5500, www.sfmohcd.org.

- 29. Unit Mix.** The Project contains 85 studios, 206 one-bedroom, and 209 two-bedroom units; therefore, the required affordable unit mix for the Inclusionary Units is 13 studios, 30 one-bedroom, and 31 two-bedroom units. The Project requires four additional units be provided at 50% AMI to qualify for a 42.5% bonus. The project sponsor has elected to provide two (2) one-bedroom units and two (2) two-bedroom units at 50% AMI. The final unit mix of the affordable units is 13 studios, 32 one-bedroom and 33 two-bedroom units. If the market-rate unit mix changes, the affordable unit

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mix will be modified accordingly with written approval from Planning Department staff in consultation with MOHCD.

For information about compliance, contact the Case Planner, Planning Department at (628) 652-7600, www.sfplanning.org or the Mayor's Office of Housing and Community Development at (415) 701-5500, www.sfmohcd.org.

- 30. Mixed Income Levels for Affordable Units.** Pursuant to Planning Code Section 415.3, the Project is required to provide 21% of the proposed dwelling units in the base density project as affordable to qualifying households. At least The base project contains 352 units; therefore, 74 units are required. At least 12% must be affordable to low-income households, at least 4.5% must be affordable to moderate income households, and at least 4.5% must be affordable to middle income households. Rental Units for low-income households shall have an affordable rent set at 55% of Area Median Income or less, with households earning up to 65% of Area Median Income eligible to apply for low-income units. Rental Units for moderate-income households shall have an affordable rent set at 80% of Area Median Income or less, with households earning from 65% to 90% of Area Median Income eligible to apply for moderate-income units. Rental Units for middle-income households shall have an affordable rent set at 110% of Area Median Income or less, with households earning from 90% to 130% of Area Median Income eligible to apply for middle-income units. For any affordable units with rental rates set at 110% of Area Median Income, the units shall have a minimum occupancy of two persons. In order to qualify for the 42.5% bonus, at least 13% of units must be provided to very low-income households; therefore, the project is providing four (4) additional affordable units at 50% AMI, as defined in CA Govt. Code Section 65915. If the number of market-rate units change, the number of required affordable units shall be modified accordingly with written approval from Planning Department staff in consultation with the Mayor's Office of Housing and Community Development ("MOHCD").

For information about compliance, contact the Case Planner, Planning Department at (628) 652-7600, www.sfplanning.org or the Mayor's Office of Housing and Community Development at (415) 701-5500, www.sfmohcd.org.

- 31. Minimum Unit Sizes.** The affordable units shall meet the minimum unit sizes standards established by the California Tax Credit Allocation Committee (TCAC) as of May 16, 2017. One-bedroom units must be at least 450 square feet, two-bedroom units must be at least 700 square feet, and three-bedroom units must be at least 900 square feet. Studio units must be at least 300 square feet pursuant to Planning Code Section 415.6(f)(2). The total residential floor area devoted to the affordable units shall not be less than the applicable percentage applied to the total residential floor area of the principal project, provided that a 10% variation in floor area is permitted.

For information about compliance, contact the Case Planner, Planning Department at (628) 652-7600, www.sfplanning.org or the Mayor's Office of Housing and Community Development at (415) 701-5500, www.sfmohcd.org.

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- 32. Conversion of Rental Units:** In the event one or more of the Rental Units are converted to Ownership units, the project sponsor shall either (A) reimburse the City the proportional amount of the inclusionary affordable housing fee, which would be equivalent to the then-current inclusionary affordable fee requirement for Owned Units, or (B) provide additional on-site or off-site affordable units equivalent to the difference between the on-site rate for rental units approved at the time of entitlement and the then-current inclusionary requirements for Owned Units, The additional units shall be apportioned among the required number of units at various income levels in compliance with the requirements in effect at the time of conversion.

For information about compliance, contact the Case Planner, Planning Department at (628) 652-7600, www.sfplanning.org or the Mayor's Office of Housing and Community Development at (415) 701-5500, www.sfmohcd.org.

- 33. Notice of Special Restrictions.** The affordable units shall be designated on a reduced set of plans recorded as a Notice of Special Restrictions on the property prior to architectural addenda. The designation shall comply with the designation standards published by the Planning Department and updated periodically.

For information about compliance, contact the Case Planner, Planning Department at (628) 652-7600, www.sfplanning.org or the Mayor's Office of Housing and Community Development at (415) 701-5500, www.sfmohcd.org.

- 34. Duration.** Under Planning Code Section 415.8, all units constructed pursuant to Section 415.6, must remain affordable to qualifying households for the life of the project.

For information about compliance, contact the Case Planner, Planning Department at (628) 652-7600, www.sfplanning.org or the Mayor's Office of Housing and Community Development at (415) 701-5500, www.sfmohcd.org.

- 35. 20% Below Market Rents.** Pursuant to Planning Code Section 415.6, the maximum affordable rents shall be no higher than 20% below market rents for the neighborhood within which the project is located, which shall be defined in accordance with the American Community Survey Neighborhood Profile Boundaries Map. MOHCD shall adjust the allowable rents, and the eligible households for such units, accordingly, and such potential readjustment shall be a condition of approval upon project entitlement. The City shall review the updated data on neighborhood rents and sales prices on an annual basis

- 36. Other Conditions.** The Project is subject to the requirements of the Inclusionary Affordable Housing Program under Section 415 et seq. of the Planning Code and City and County of San Francisco Inclusionary Affordable Housing Program Monitoring and Procedures Manual ("Procedures Manual"). The Procedures Manual, as amended from time to time, is incorporated

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herein by reference, as published and adopted by the Planning Commission, and as required by Planning Code Section 415. Terms used in these conditions of approval and not otherwise defined shall have the meanings set forth in the Procedures Manual. A copy of the Procedures Manual can be obtained at the MOHCD at 1 South Van Ness Avenue or on the Planning Department or MOHCD websites, including on the internet at:

<http://sf-planning.org/Modules/ShowDocument.aspx?documentid=4451>

As provided in the Inclusionary Affordable Housing Program, the applicable Procedures Manual is the manual in effect at the time the subject units are made available for sale.

For information about compliance, contact the Case Planner, Planning Department at (628) 652-7600, www.sfplanning.org or the Mayor's Office of Housing and Community Development at (415) 701-5500, www.sfmohcd.org.

- a. The affordable unit(s) shall be designated on the building plans prior to the issuance of the first construction permit by the Department of Building Inspection ("DBI"). The affordable unit(s) shall (1) be constructed, completed, ready for occupancy and marketed no later than the market rate units, and (2) be evenly distributed throughout the building; and (3) be of comparable overall quality, construction and exterior appearance as the market rate units in the principal project. The interior features in affordable units should be generally the same as those of the market units in the principal project, but need not be the same make, model or type of such item as long they are of good and new quality and are consistent with then-current standards for new housing. Other specific standards for on-site units are outlined in the Procedures Manual.
- b. If the units in the building are offered for rent, the affordable unit(s) shall be rented to qualifying households. The initial and subsequent rent level of such units shall be calculated according to the Procedures Manual. Limitations on (i) occupancy; (ii) lease changes; (iii) subleasing, and; are set forth in the Inclusionary Affordable Housing Program and the Procedures Manual.
- c. The affordable units that satisfy both the Density Bonus Law and the Inclusionary Affordable Housing Program shall be rented to very low-income households, as defined as households earning 50% of AMI in the California Health and Safety Code Section 50105 and/or California Government Code Sections 65915-65918, the State Density Bonus Law. The income table used to determine the rent and income levels for the Density Bonus units shall be the table required by the State Density Bonus Law. If the resultant rent or income levels at 50% of AMI under the table required by the State Density Bonus Law are higher than the rent and income levels at 55% of AMI under the Inclusionary Affordable Housing Program, the rent and incomes levels shall default to the maximum allowable rent and income levels for affordable units under the Inclusionary Affordable Housing Program. After such Density Bonus Law units have been rented for a term of 55 years, the subsequent rent and income levels of such units may be adjusted to (55) percent of Area Median Income under the Inclusionary Affordable Housing Program, using income table called "Maximum Income by Household Size derived from the Unadjusted Area Median Income for HUD Metro Fair Market Rent Area that contains San Francisco," and shall remain affordable for the remainder of the life of the Project. The initial and subsequent rent level of such units shall be calculated according to the Procedures Manual. The remaining units

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being offered for rent shall be rented to qualifying households, as defined in the Procedures Manual, whose gross annual income, adjusted for household size, does not exceed an average fifty-five (55) percent of Area Median Income under the income table called “Maximum Income by Household Size derived from the Unadjusted Area Median Income for HUD Metro Fair Market Rent Area that contains San Francisco.” The initial and subsequent rent level of such units shall be calculated according to the Procedures Manual. Limitations on (i) occupancy; (ii) lease changes; and (iii) subleasing are set forth in the Inclusionary Affordable Housing Program and the Procedures Manual.

- d. The Project Sponsor is responsible for following the marketing, reporting, and monitoring requirements and procedures as set forth in the Procedures Manual. MOHCD shall be responsible for overseeing and monitoring the marketing of affordable units. The Project Sponsor must contact MOHCD at least six months prior to the beginning of marketing for any unit in the building.
- e. Required parking spaces shall be made available to initial buyers or renters of affordable units according to the Procedures Manual.
- f. Prior to the issuance of the first construction permit by DBI for the Project, the Project Sponsor shall record a Notice of Special Restriction on the property that contains these conditions of approval and a reduced set of plans that identify the affordable units satisfying the requirements of this approval. The Project Sponsor shall promptly provide a copy of the recorded Notice of Special Restriction to the Department and to MOHCD or its successor.
- g. If the Project Sponsor fails to comply with the Inclusionary Affordable Housing Program requirement, the Director of DBI shall deny any and all site or building permits or certificates of occupancy for the development project until the Planning Department notifies the Director of compliance. A Project Sponsor’s failure to comply with the requirements of Planning Code Section 415 et seq. shall constitute cause for the City to record a lien against the development project and to pursue any and all available remedies at law, including penalties and interest, if applicable.

Monitoring - After Entitlement

- 37. Enforcement.** Violation of any of the Planning Department conditions of approval contained in this Motion or of any other provisions of Planning Code applicable to this Project shall be subject to the enforcement procedures and administrative penalties set forth under Planning Code Section 176 or Section 176.1. The Planning Department may also refer the violation complaints to other city departments and agencies for appropriate enforcement action under their jurisdiction.

For information about compliance, contact Code Enforcement, Planning Department at 628.652.7316, www.sfplanning.org

- 38. Monitoring.** The Project requires monitoring of the conditions of approval in this Motion. The Project Sponsor or the subsequent responsible parties for the Project shall pay fees as established under Planning Code Section 351(e) (1) and work with the Planning Department for information about compliance.

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For information about compliance, contact Code Enforcement, Planning Department at 628.652.7316, www.sfplanning.org

Operation

- 39. Sidewalk Maintenance.** The Project Sponsor shall maintain the main entrance to the building and all sidewalks abutting the subject property in a clean and sanitary condition in compliance with the Department of Public Works Streets and Sidewalk Maintenance Standards.

For information about compliance, contact Bureau of Street Use and Mapping, Department of Public Works, 628.271.2000, www.sfpublishworks.org

- 40. Community Liaison.** Prior to issuance of a building permit to construct the project and implement the approved use, the Project Sponsor shall appoint a community liaison officer to deal with the issues of concern to owners and occupants of nearby properties. The Project Sponsor shall provide the Zoning Administrator and all registered neighborhood groups for the area with written notice of the name, business address, and telephone number of the community liaison. Should the contact information change, the Zoning Administrator and registered neighborhood groups shall be made aware of such change. The community liaison shall report to the Zoning Administrator what issues, if any, are of concern to the community and what issues have not been resolved by the Project Sponsor.

For information about compliance, contact Code Enforcement, Planning Department at 628.652.7316, www.sfplanning.org

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The use of said property contrary to these special restrictions shall constitute a violation of the Planning Code, and no release, modification or elimination of these restrictions shall be valid unless notice thereof is recorded on the Land Records by the Zoning Administrator of the City and County of San Francisco; except that in the event that the zoning standards above are modified so as to be less restrictive and the uses therein restricted are thereby permitted and in conformity with the provisions of the Planning Code. This document would no longer be in effect and would be null and void.

_____(Signature)
_____(Print Name)

Dated: _____, 21 at _____, **California.**
(Month, Day) (City)

_____(Signature)
_____(Print Name)

Dated: _____, 21 at _____, **California.**
(Month, Day) (City)

_____(Signature)
_____(Print Name)

Dated: _____, 21 at _____, **California.**
(Month, Day) (City)

Each signature must be acknowledged by a notary public before recordation; add Notary Public Certification(s) and Official Notarial Seal(s) below.

NOTICE OF SPECIAL RESTRICTIONS UNDER THE PLANNING CODE

EXHIBIT A – Property Description

555-585 BRYANT STREET HOUSING SUSTAINABILITY DISTRICT MITIGATION COMPLIANCE AND AGREEMENT TO IMPLEMENT MITIGATION MONITORING AND REPORTING PROGRAM

<i>Record No.:</i>	2021-000947PRJ	<i>Block/Lot:</i>	3776/034, 038, 039, 040, 042, 043, and 044
<i>Project Title:</i>	555-585 Bryant Street	<i>Lot Size:</i>	33,000 square feet
<i>BPA Nos.:</i>	2021-02124635	<i>Project Sponsor:</i>	Will Goodman, Strada
<i>Zoning:</i>	Central South of Market Mixed Use Office (CMUO) 130-S Height and Bulk District	<i>Lead Agency:</i>	San Francisco Planning Department
		<i>Staff Contact:</i>	Elizabeth White, 628-652-7557

Project Description

The proposed project would combine seven lots between Bryant and Welsh streets mid-block between Fourth and Zoe streets, demolish the existing industrial buildings, and construct a 160-foot-tall (up to 176 feet tall with rooftop appurtenances) mixed-used building. The proposed 16-story building would include approximately 434,000 square feet of residential use and 22,000 square feet of production, distribution, and repair use. The 500-dwelling unit building would be composed of 74 studios, 228 one-bedroom units, and 198 two-bedroom units. Of the 500 dwelling units, 78 units would be provided as affordable housing units and 422 units would be provided as market rate housing units. The project would provide 125 accessory parking spaces, 200 class 1 bicycle parking spaces, and 32 class 2 bicycle parking spaces. The project would provide approximately 5,000 square feet of useable open space for residents and approximately 400 square feet of public open space. The project involves excavation activities to approximately 35 feet below grade and the removal of approximately 36,000 cubic yards of soil.

Compliance with Central South of Market (SoMa) Programmatic Environmental Impact Report (PEIR) Mitigation Measures

The proposed project is seeking ministerial approval through the Central SoMa Housing Sustainability District process provided for in planning code section 34.3. To be eligible for approval under planning code section 34.3, the proposed project must comply with mitigation measures in the Central SoMa PEIR that the planning department determines apply to the project (section 34.3(d)(10)). The table below identifies Central SoMa mitigation measures that may apply to subsequent development projects in the plan area. The table indicates whether the mitigation measure applies to the proposed 555-585 Bryant Street project and, if a mitigation measure applies to the project, the table indicates when compliance with each mitigation measure must occur. Some mitigation measures

span multiple phases. Substantive descriptions of each applicable mitigation measure’s requirement that has yet to be complied with are provided on the following pages in the Mitigation Monitoring and Reporting Program.

Central SoMa PEIR Mitigation Measures	Applicability to the Proposed Project ¹		Period of Compliance ²				Compliance with Mitigation Measure Completed? ⁵
	Applies to the Proposed Project (Y/N?)	Discussion	Prior to the start of Construction ³	During Construction ⁴	Post-Construction or Operational		
Cultural Resources							
Central SoMa M-CP-1a: Mandatory Consultation Regarding Avoidance or Minimization of Effects on Historical Resources	N	There are no historical resources on the project site.					
Central SoMa M-CP-1b: Documentation of Historical Resources(s)	N	There are no historical resources on the project site.					
Central SoMa M-CP-1c: Oral Histories	N	There are no historical resources on the project site.					
Central SoMa M-CP-1d: Interpretive Program	N	There are no historical resources on the project site.					
Central SoMa M-CP-1e: Video Recordation	N	There are no historical resources on the project site.					
Central SoMa M-CP-3a; Project Mitigation Measure 1: Protect Historical Resources from Adjacent Construction Activities	Y	Project construction activities could affect adjacent historic resources.	X	X			

Applicability to the Proposed Project¹

Period of Compliance²

Central SoMa PEIR Mitigation Measures	Applies to the Proposed Project (Y/N?)	Discussion	Prior to the start of Construction³	During Construction⁴	Post-Construction or Operational	Compliance with Mitigation Measure Completed?⁵
Central SoMa M-CP-3b; Project Mitigation Measure 2: Construction Monitoring Program for Historical Resources	Y	Project construction activities could affect adjacent historic resources.	X	X		
Central SoMa M-CP-4a and M-CP-4b; Project Mitigation Measure 3: Archeological Testing	Y	Preliminary archeological assessment determined archeological testing is required.	X	X	X	
Tribal Cultural Resources						
Central SoMa M-CP-5; Project Mitigation Measure 4: Tribal Cultural Resources Archeological Resource Preservation Plan and/or Interpretive Program	Y	Preliminary archeological assessment determined tribal cultural resources may be affected during construction.		X	X	
Transportation and Circulation						
Central SoMa M-CP-3a; Project Mitigation Measure 5: Transit Enhancements	Y	Project includes more than 20 off-street vehicle spaces.			X	
Central SoMa M-CP-6a: Driveway and loading operations plan	Y	This mitigation measure is satisfied by compliance with planning code section 155(u) (Driveway and Loading Operations				X (completed as part of planning code compliance review)

		Applicability to the Proposed Project ¹		Period of Compliance ²			
Central SoMa PEIR Mitigation Measures	Applies to the Proposed Project (Y/N?)	Discussion	Prior to the start of Construction ³	During Construction ⁴	Post-Construction or Operational	Compliance with Mitigation Measure Completed? ⁵	
		Plan (DLOP) in Central SoMa Special Use District); since it is part of planning code compliance, further compliance through the MMRP is not required.					
Central SoMa M-CP-6b: Accommodation of on-street commercial loading spaces and passenger loading/unloading zones	Y	This mitigation measure is satisfied by compliance with planning code sections 155(u).				X (completed as part of planning code compliance review)	
Central SoMa M-CP-8: Emergency Vehicle Access Consultation	N	The project would not reduce the number of available vehicle travel lanes for a total distance of more than one block where transit-only lanes are not provided.					
Central SoMa M-TR-9; Project Mitigation Measure 6: Construction Management Plan and Construction Coordination	Y	Project construction at the site could overlap in time with nearby construction projects, requiring coordination.	X	X			

Applicability to the Proposed Project¹

Period of Compliance²

Central SoMa PEIR Mitigation Measures	Applies to the Proposed Project (Y/N?)	Discussion	Prior to the start of Construction³	During Construction⁴	Post-Construction or Operational	Compliance with Mitigation Measure Completed?⁵
Central SoMa M-NO-1a: Transportation Demand Management for New Development Projects	Y	This mitigation measure is satisfied by compliance with planning code section 169.				X (completed as part of planning code compliance review)
Noise						
Central SoMa M-NO-1b; Project Mitigation Measure 7: Siting of Noise-Generating Uses	Y	Project includes new stationary noise sources.				X (noise control feature shown on the May 27, 2021 plan set, pages A-047, A-048, A-049, and A-050)
Central SoMa M-NO-2a; Project Mitigation Measure 8: General Construction Noise Control Measures	Y	Project construction activities would occur within 100 feet of noise sensitive receptors.	X	X		
Central SoMa M-NO-2b: Noise and Vibration Control Measures During Pile Driving	N	Project does not include or require driven piles.				
Air Quality						
Central SoMa M-AQ-3a: Education for Residential and Commercial Tenants Concerning Low-VOC Consumer Products	N	Project does not exceed operational criteria air pollutant screening sizes and is therefore presumed to				

Applicability to the Proposed Project¹

Period of Compliance²

Central SoMa PEIR Mitigation Measures	Applies to the Proposed Project (Y/N?)	Discussion	Prior to the start of Construction ³	During Construction ⁴	Post-Construction or Operational	Compliance with Mitigation Measure Completed? ⁵
		not result in significant operational criteria air pollutant emissions; therefore, this measure does not apply to the project.				
Central SoMa M-AQ-3b: Reduce Operational Emissions	N	Project does not exceed operational criteria air pollutant screening sizes and is therefore presumed to not result in significant operational criteria air pollutant emissions; therefore, this measure does not apply to the project				
Central SoMa M-AQ-4a: Construction Emissions Minimization Analysis	N	This measure is intended to determine if M-AQ-4b is required. The project is required to comply with M-AQ-4b, so additional analysis is not required.				
Central SoMa M-AQ-4b & 6a: Project Mitigation Measure 10: Construction Emissions Minimization Plan (Implementation of M-AQ-	Y	The project would result in construction-related diesel	X	X		

Applicability to the Proposed Project ¹		Period of Compliance ²				
Central SoMa PEIR Mitigation Measures	Applies to the Proposed Project (Y/N/?)	Discussion	Prior to the start of Construction ³	During Construction ⁴	Post-Construction or Operational	Compliance with Mitigation Measure Completed? ⁵
4b: Construction Emissions Minimization Plan)		particulate matter emissions that affect sensitive receptors within the air pollutant exposure zone.				
Central SoMa M-AQ-5a; Project Mitigation Measure 9: Best Available Control Technology for Diesel Generators and Fire Pumps	Y	The proposed project requires a diesel generator. Diesel particulate matter from the generator may affect sensitive receptors within the air pollutant exposure zone. The project requires a fire pump which will be electrically powered.			X	(Tier 4 generator shown on the May 27, 2021 plan set on page A-047 and electric fire pump shown on page A-041)
Central SoMa M-AQ-5b: Siting of Uses that Emit Particulate Matter (PM _{2.5}), Diesel Particulate Matter, or Other Toxic Air Contaminants.	N	Proposed PDR use is a trade shop (retail and services that provide custom-crafted goods and/or services). No known PM _{2.5} diesel particulate matter or other toxic air contaminants are proposed.				

Applicability to the Proposed Project¹

Period of Compliance²

Central SoMa PEIR Mitigation Measures	Applies to the Proposed Project (Y/N/?)	Discussion	Prior to the start of Construction ³	During Construction ⁴	Post-Construction or Operational	Compliance with Mitigation Measure Completed? ⁵
<p>Central SoMa M-AQ-5d; Project Mitigation Measure 11: Land Use Buffers Around Active Loading Docks</p>	Y	Project includes two service vehicle bays and one on street truck loading zone				<p>X (the truck loading zone is located at street level approximately 250 feet from the sensitive receptor at 504 4th Street and the two service vehicle bays are located in the basement and sufficiently buffered from sensitive land uses shown on the May 27, 2021 plan set, page A-043)</p>
Wind						
<p>Central SoMa M-WI-1; Project Mitigation Measure 12: Wind Hazard Criterion for the Plan Area</p>	Y	The proposed project is taller than 85 feet and requires a wind hazard analysis			X	<p>X⁶ (as documented in the June 24, 2021 pedestrian wind study; maintenance of landscaping required to reduce pedestrian wind speeds is required in perpetuity)</p>
<p>Central SoMa M-BI-1; Project Mitigation Measure 13: Pre-construction Bat Survey</p>	Y	The existing buildings are currently occupied and not anticipated to be vacant for more than 6 months, however, the proposed project involves tree removal.	X	X		

Applicability to the Proposed Project¹

Period of Compliance²

Central SoMa PEIR Mitigation Measures	Applies to the Proposed Project (Y/N/?)	Discussion	Prior to the start of Construction ³	During Construction ⁴	Post-Construction or Operational	Compliance with Mitigation Measure Completed? ⁵
Hazardous Materials Central SoMa M-HZ-3: Hazardous Building Materials Abatement	N	Regulations are in place to address the proper removal and disposal of hazardous building materials, including asbestos-containing building materials and lead-based paint.				

Notes:

1. These columns list all the mitigation measures in the Central SoMa PEIR that may apply to subsequent development projects in the Central SoMa Plan Area. The mitigation measure's applicability to the proposed project is identified along with a brief explanation.
2. These columns are completed only for mitigation measures that the department determines apply to the proposed project.
3. Prior to any ground disturbing activities at the project site.
4. Construction is broadly defined to include any physical activities associated with construction of a development project, including but not limited to: site preparation, clearing, demolition, excavation, shoring, foundation installation, and building construction.
5. Measures that have been complied with are shown in this table and compliance with the measure is indicated in the right-hand column. With the exception of Project Mitigation Measures 7 and 11, the description of these mitigation measures are not included in the Mitigation Monitoring and Reporting program below. Mitigation Measures 7 and 11 are included in the Mitigation Monitoring and Reporting Program because these measures require ongoing compliance throughout project operations.
6. A pedestrian wind study has been prepared in compliance with Central SoMa Mitigation Measure M-WI-1. The wind study concludes that under existing plus project and project plus cumulative conditions, the proposed project would result in an exceedance of the 1-hour wind hazard criterion. Under existing plus project conditions, 2 locations exceed the 1-hour wind hazard criterion for a total duration of 6 hours. Under project plus cumulative conditions, 6 locations exceed the 1-hour wind hazard criterion for a total duration of 71 hours. Because the proposed project does not meet the 1-hour wind hazard criterion under existing plus project or project plus cumulative conditions, all feasible mitigation measures have been incorporated in the proposed project. These measures include: six Lophostemon confertus street trees on Bryant Street and 8 Tristaniopsis laurina street trees on Welsh Street; the height and density of these trees are assumed to be 15-20 feet in height and have canopy widths of 8-10 feet. These measures are shown on the May 27, 2021 plan set on pages A-043 and L-01. With incorporation of the existing and proposed landscaping in the wind tunnel test, the number of wind hazard locations have been reduced to 1 for a total duration of 2 hours. The pedestrian wind study investigated other feasible wind control measures that included canopies and vertical fins of various configurations attached to the building façade. The results of the wind study conclude that these additional wind control measures would not provide substantial additional reduction in pedestrian level wind currents that would further reduce the wind hazard. Therefore, in compliance with Central SoMa Mitigation Measure M-WI-1, the proposed project has incorporated all feasible mitigation measures to reduce the location and number of hours of wind hazard exceedances. The remainder of Mitigation Measure M-WI-1 requires that the proposed landscaping be maintained in perpetuity.

	Applicability to the Proposed Project ¹		Period of Compliance ²			
Central SoMa PEIR Mitigation Measures	Applies to the Proposed Project (Y/N/?)	Discussion	Prior to the start of Construction ³	During Construction ⁴	Post-Construction or Operational	Compliance with Mitigation Measure Completed? ⁵
Central SoMa Improvement Measures						

	Applicability to the Proposed Project ¹		Period of Compliance ²			
Central SoMa Improvement Measures	Applies to the Proposed Project (Y/N)?	Discussion	Prior to the start of Construction ³	During Construction ⁴	Post-Construction or Operational	Compliance with Improvement Measure completed? ⁵
Project Improvement Measure 1: Night Lighting Minimization	Y	Proposed uses would include new sources of nighttime lighting			X	

Notes:

1. These columns list the improvement measure in the Central SoMa PEIR that may apply to subsequent development projects in the Central SoMa Plan Area. The improvement measure's applicability to the proposed project is identified along with a brief explanation.
2. These columns are completed only for the improvement measure that the department determines apply to the proposed project.
3. Prior to any ground disturbing activities at the project site.
4. Construction is broadly defined to include any physical activities associated with construction of a development project, including but not limited to: site preparation, clearing, demolition, excavation, shoring, foundation installation, and building construction.
5. Improvement measures that have been compiled with are shown in this table and compliance with the measure is indicated in the right-hand column. Improvement measures that have been compiled with are not included in the Mitigation Monitoring and Reporting program below.

I agree to implement the attached mitigation and improvement measures as a condition of project approval.

06/28/21

Property Owner or Legal Agent Signature

Date

Note to sponsor: Please contact CPC.EnvironmentalMonitoring@sfgov.org to begin the environmental monitoring process prior to the submittal of your building permits to the San Francisco Department Building Inspection.

Determination

The planning department determines the above mitigation measures from the Central SoMa PEIR apply to the proposed project.

Lisa Gibson, Environmental Review Officer

6/29/21

Date

MITIGATION MONITORING AND REPORTING PROGRAM

MONITORING AND REPORTING PROGRAM¹

Adopted Mitigation Measures	Implementation Responsibility	Mitigation Schedule	Monitoring / Reporting Responsibility	Monitoring Actions / Completion Criteria
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MITIGATION MEASURES AGREED TO BY PROJECT SPONSOR

CULTURAL RESOURCES

Project Mitigation Measure 1 (Implementation of Central SoMa PEIR M-CP-

3a): Protect Historical Resources from Adjacent Construction Activities

The project sponsor shall incorporate into construction specifications for the proposed project a requirement that the construction contractor(s) use all feasible means to avoid damage to the 539 Bryant Street building (located on block 3776, lot 41), a historic resource. Such methods may include maintaining a safe distance between the construction site and the 539 Bryant Street building, using construction techniques that reduce vibration (such as using concrete saws instead of jackhammers or hoe-rams to open excavation trenches, the use of non-vibratory rollers, and hand excavation), appropriate excavation shoring methods to prevent movement of adjacent structures, and providing adequate security to minimize risks of vandalism and fire.

Project sponsor and qualified historic preservation individual.

Prior the start of any demolition, construction, or earth movement.

Planning Department Preservation Technical Specialist and project sponsor.

Considered complete upon acceptance by Planning Department of affidavit from project sponsor that construction specifications to avoid damage to the 539 Bryant Street building have been incorporated into the project.

Project sponsor and qualified historic preservation individual.

Project Mitigation Measure 2 (Implementation of Central SoMa PEIR M-CP-3b): Construction Monitoring Program for Historical Resources

The project sponsor shall undertake a monitoring program to minimize damage to the 539 Bryant Street building (located on block 3776, lot 41), a historic building, and to ensure that any such damage is documented and repaired. The monitoring program shall apply within 25 feet, and shall include the following components, subject to access being granted by the owner(s) of 539 Bryant Street. Prior to the start of any ground-disturbing activity, the project sponsor shall engage a historic architect or qualified historic preservation professional to undertake a preconstruction survey of 539 Bryant Street and photograph the building's existing conditions. Based on the construction and condition of the resource, the consultant shall also establish a standard maximum vibration level that shall not be exceeded at the 539 Bryant Street building, based on existing

Project sponsor and construction contractor.

Prior to the issuance of construction permits, the project sponsor shall submit the following for approval by the Planning Department:

- Pre-construction survey of 539 Bryant Street
- Vibration management and monitoring plan identifying

Preservation Technical Specialist and project sponsor

Considered complete upon submittal to Planning Department of post-construction Vibration Monitoring Results report documenting monitoring activities, effects, if any, on 539 Bryant Street, and measures taken to remediate any damage.

MONITORING AND REPORTING PROGRAM¹

Adopted Mitigation Measures	Implementation Responsibility	Mitigation Schedule	Monitoring / Reporting Responsibility	Monitoring Actions / Completion Criteria
<p>condition, character defining features, soils conditions, and anticipated construction practices (a common standard is 0.2 inch per second, peak particle velocity). To ensure that vibration levels do not exceed the established standard, the project sponsor shall monitor vibration levels at the structure and shall prohibit vibratory construction activities that generate vibration levels in excess of the standard. Should owner permission not be granted, the project sponsor shall employ alternative methods of vibration monitoring in areas under control of the project sponsor. Should vibration levels be observed in excess of the standard, construction shall be halted and alternative construction techniques put in practice, to the extent feasible. (For example, the project will use pre-drilled piles instead of driven piles, if feasible based on soils conditions; smaller, lighter equipment might be able to be used in some cases.) The consultant shall conduct regular periodic inspections of 539 Bryant Street during ground-disturbing activity on the project site. Should damage to the 539 Bryant Street building occur, the building shall be remediated to its pre-construction condition at the conclusion of ground-disturbing activity on the site.</p>	<p>Project sponsor's qualified archeological consultant and construction contractor</p>	<p>maximum vibration levels not to be exceeded, monitoring plan, and alternative construction techniques to be implemented in the event maximum vibration levels are exceeded.</p>	<p>Environmental Review Officer and project sponsor</p>	<p>Considered complete after Final Archeological Resources Report is approved.</p>
<p>Project Mitigation Measure 3 (Implementation of Central SoMa PEIR Mitigation Measure M-CP-4a): Archeological Testing The project sponsor shall retain the services of an archeological consultant from the rotational qualified archeological consultants list (QACL) maintained by the planning department. After the first project approval action or as directed by the Environmental Review Officer (ERO), the project sponsor shall contact the department archeologist to obtain the names and contact information for the next three archeological consultants on the QACL.</p>	<p>Project sponsor's qualified archeological consultant and construction contractor</p>	<p>Prior to issuance of construction permits and throughout the construction period.</p>	<p>Environmental Review Officer and project sponsor</p>	<p>Considered complete after Final Archeological Resources Report is approved.</p>
<p>The archeological consultant shall undertake an archeological testing program as specified herein. In addition, the consultant shall be available to conduct an archeological monitoring and/or data recovery program if required pursuant to this measure. The archeological consultant's work shall be conducted in accordance with this measure at the direction of the Environmental Review Officer (ERO). All plans and reports prepared by the consultant as specified herein shall be submitted first and directly to the ERO for review and comment and shall be considered draft reports subject to revision until final approval by the ERO. Archeological monitoring and/or data recovery programs required by this measure could suspend construction of the project for up to a maximum of four</p>				

MONITORING AND REPORTING PROGRAM¹

	Adopted Mitigation Measures	Implementation Responsibility	Mitigation Schedule	Monitoring / Reporting Responsibility	Monitoring Actions / Completion Criteria
	<p>weeks. At the direction of the ERO, the suspension of construction can be extended beyond four weeks only if such a suspension is the only feasible means to reduce to a less than significant level potential effects on a significant archeological resource as defined in CEQA Guidelines Sect. 15064.5 (a)(c).</p>				
	<p><u>Archeological Testing Program.</u> The purpose of the archeological testing program will be to determine to the extent possible the presence or absence of archeological resources and to identify and to evaluate whether any archeological resource encountered on the site constitutes an historical resource under CEQA.</p>	<p>Project sponsor's qualified archeological consultant and construction contractor</p>	<p>Prior to issuance of construction permits and throughout the construction period.</p>	<p>Planning Department</p>	<p>Considered complete after approval of Archeological Testing Plan.</p>
	<p>The archeological testing program shall be conducted in accordance with the approved Archeological Testing Plan (ATP). The archeological consultant and the ERO shall consult on the scope of the ATP, which shall be approved by the ERO prior to any project-related soils disturbing activities commencing. The ATP shall be submitted first and directly to the ERO for review and comment and shall be considered a draft subject to revision until final approval by the ERO. The archeologist shall implement the testing as specified in the approved ATP prior to and/or during construction.</p>				
	<p>The ATP shall identify the property types of the expected archeological resource(s) that potentially could be adversely affected by the proposed project, lay out what scientific/historical research questions are applicable to the expected resource, what data classes the resource is expected to possess, and how the expected data classes would address the applicable research questions. The ATP shall also identify the testing method to be used, the depth or horizontal extent of testing, and the locations recommended for testing and shall identify archeological monitoring requirements for construction soil disturbance as warranted.</p>				
	<p><u>Discovery Treatment Determination.</u> At the completion of the archeological testing program, the archeological consultant shall submit a written summary of the findings to the ERO. The findings memo shall describe and identify each resource and provide an initial assessment of the integrity and significance of encountered archeological deposits.</p>	<p>The archeological consultant, Project Sponsor and project contractor at the</p>	<p>At the completion of archeological testing or discovery of a potentially significant</p>	<p>Planning Department/project sponsor</p>	<p>If preservation in place is feasible, complete when approved ARPP is implemented.</p>

MONITORING AND REPORTING PROGRAM¹

Adopted Mitigation Measures	Implementation Responsibility	Mitigation Schedule	Monitoring / Reporting Responsibility	Monitoring Actions / Completion Criteria
<p>If the ERO in consultation with the archaeological consultant determines that a significant archaeological resource is present and that the resource could be adversely affected by the proposed project, the ERO, in consultation with the project sponsor, shall determine whether preservation of the resource in place is feasible. If so, the proposed project shall be re-designed so as to avoid any adverse effect on the significant archaeological resource and the archaeological consultant shall prepare an archaeological resource preservation plan (ARPP), which shall be implemented by the project sponsor during construction. The consultant shall submit a draft ARPP to the planning department for review and approval.</p> <p>If preservation in place is not feasible, a data recovery program shall be implemented, unless the ERO determines that the archaeological resource is of greater interpretive than research significance and that interpretive use of the resource is feasible. The ERO in consultation with the archaeological consultant shall also determine if additional treatment is warranted, which may include additional testing and/or construction monitoring.</p>	<p>direction of the ERO.</p>	<p>archeological resource</p>		<p>If preservation in place is not feasible, complete when treatment is determined and implemented.</p>
<p><u>Consultation with Descendant Communities.</u> On discovery of an archaeological site associated with descendant Native Americans, the Overseas Chinese, or other potentially interested descendant group an appropriate representative of the descendant group and the ERO shall be contacted. The representative of the descendant group shall be given the opportunity to monitor archaeological field investigations of the site and to offer recommendations to the ERO regarding appropriate archaeological treatment of the site, of recovered data from the site, and, if applicable, any interpretive treatment of the associated archaeological site. A copy of the Archaeological Resources Report (ARR) shall be provided to the representative of the descendant group.</p>	<p>The archaeological consultant, Project Sponsor and project contractor at the direction of the ERO.</p>	<p>During testing and if applicable monitoring of soils disturbing activities.</p>	<p>Planning Department/project sponsor</p>	<p>Descendant group provides recommendations and is given a copy of the ARR.</p>
<p><u>Archaeological Data Recovery Plan.</u> An archaeological data recovery program shall be conducted in accordance with an Archaeological Data Recovery Plan (ADRP) if all three of the following apply: 1) a resource has potential to be significant, 2) preservation in place is not feasible, and 3) the ERO determines that an archaeological data recovery program is warranted. The archaeological consultant, project sponsor, and ERO shall meet and consult on the scope of the ADRP prior</p>	<p>Project sponsor's qualified archeological consultant and contractor.</p>	<p>In the event that an archeological site is uncovered during the construction period.</p>	<p>Planning Department/project sponsor</p>	<p>Considered complete after approval of Final Archeological Results Report.</p>

MONITORING AND REPORTING PROGRAM¹

Adopted Mitigation Measures	Implementation Responsibility	Mitigation Schedule	Monitoring / Reporting Responsibility	Monitoring Actions / Completion Criteria
<p>to preparation of a draft ADRP. The archaeological consultant shall submit a draft ADRP to the ERO. The ADRP shall identify how the proposed data recovery program will preserve the significant information the archaeological resource is expected to contain. That is, the ADRP will identify what scientific/historical research questions are applicable to the expected resource, what data classes the resource is expected to possess, and how the expected data classes would address the applicable research questions. Data recovery, in general, should be limited to the portions of the historical property that could be adversely affected by the proposed project. Destructive data recovery methods shall not be applied to portions of the archaeological resources if nondestructive methods are practical.</p>				

The scope of the ADRP shall include the following elements:

- *Field Methods and Procedures.* Descriptions of proposed field strategies, procedures, and operations.
- *Cataloguing and Laboratory Analysis.* Description of selected cataloguing system and artifact analysis procedures.
- *Discard and Deaccession Policy.* Description of and rationale for field and post-field discard and deaccession policies.
- *Interpretive Program.* Consideration of an on-site/off-site public interpretive program based on the results of the archaeological data recovery program.
- *Security Measures.* Recommended security measures to protect the archaeological resource from vandalism, looting, and non-intentionally damaging activities.
- *Final Report.* Description of proposed report format and distribution of results.
- *Curation.* Description of the procedures and recommendations for the curation of any recovered data having potential research value, identification of appropriate curation facilities, and a summary of the accession policies of the curation facilities.

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Adopted Mitigation Measures	Implementation Responsibility	Mitigation Schedule	Monitoring / Reporting Responsibility	Monitoring Actions / Completion Criteria
<p><u>Human Remains and Funerary Objects</u>: The treatment of human remains and funerary objects discovered during any soils disturbing activity shall comply with applicable State and federal laws. This shall include immediate notification of the Medical Examiner of the City and County of San Francisco and, in the event of the Medical Examiner's determination that the human remains are Native American remains, notification of the California State Native American Heritage Commission, which will appoint a Most Likely Descendant (MLD). The MLD will complete his or her inspection of the remains and make recommendations or preferences for treatment within 48 hours of being granted access to the site (Public Resources Code section 5097.98). The ERO also shall be notified immediately upon the discovery of human remains.</p>	<p>Project sponsor / archeological consultant in consultation with the San Francisco Medical Examiner, NAHC, and MLD.</p>	<p>In the event that human remains are uncovered during the construction period.</p>	<p>Planning Department</p>	<p>Considered complete after approval of Final Archeological Results Report and disposition of human remains has occurred as specified in Agreement.</p>

The project sponsor and ERO shall make all reasonable efforts to develop a Burial Agreement ("Agreement") with the MLD, as expeditiously as possible, for the treatment and disposition, with appropriate dignity, of human remains and funerary objects (as detailed in CEQA Guidelines section 15064.5(d)). The Agreement shall take into consideration the appropriate excavation, removal, recordation, scientific analysis, custodianship, curation, and final disposition of the human remains and funerary objects. If the MLD agrees to scientific analyses of the remains and/or funerary objects, the archeological consultant shall retain possession of the remains and associated or unassociated funerary objects until completion of any such analyses, after which the remains and associated or unassociated funerary objects shall be reinterred or curated as specified in the Agreement.

Nothing in existing State regulations or in this mitigation measure compels the project sponsor and the ERO to accept treatment recommendations of the MLD. However, if the ERO, project sponsor and MLD are unable to reach an Agreement on scientific treatment of the remains and associated or unassociated funerary objects, the ERO, with cooperation of the project sponsor, shall ensure that the remains and funerary objects are stored securely and respectfully until they can be reinterred on the property, with appropriate dignity, in a location not subject to further or future subsurface disturbance.

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	Implementation Responsibility	Mitigation Schedule	Monitoring / Reporting Responsibility	Monitoring Actions / Completion Criteria
<p>Treatment of historic-period human remains and funerary objects discovered during any soil-disturbing activity, additionally, shall follow protocols laid out in the project's Archeological treatment documents, and in any related agreement established between the project sponsor, Medical Examiner and the ERO.</p>				
<p><u>Archeological Public Interpretation Plan.</u> The project archeological consultant shall submit an Archeological Public Interpretation Plan (APIP) if a significant archeological resource is discovered during a project. If the resource to be interpreted is a tribal cultural resource, the APIP shall be prepared in consultation with and developed with the participation of Ohlone tribal representatives. The APIP shall describe the interpretive product(s), locations or distribution of interpretive materials or displays, the proposed content and materials, the producers or artists of the displays or installation, and a long-term maintenance program. The APIP shall be sent to the ERO for review and approval. The APIP shall be implemented prior to occupancy of the project.</p>	<p>Archeological consultant at the direction of the ERO will prepare APIP. Measure laid out in APIP are implemented by sponsor and consultant.</p>	<p>Following completion of treatment, analysis, and interpretation of by archeological consultant.</p>	<p>Planning Department/project sponsor.</p>	<p>APIP is complete after review and approval by ERO. Interpretive program is complete on certification to ERO that program has been implemented.</p>
<p><u>Archeological Resources Report.</u> Whether or not significant archeological resources are encountered, the archeological consultant shall submit a written report of the findings of the testing program to the ERO. The archeological consultant shall submit a draft Archeological Resources Report (ARR) to the ERO that evaluates the historical significance of any discovered archeological resource and describes the archeological, historical research methods employed in the archeological testing/monitoring/data recovery program(s) undertaken, and if applicable, discusses curation arrangements. Formal site recordation forms (CA DPR 523 series) shall be attached to the ARR as an appendix.</p>	<p>Archeological consultant at the direction of the ERO.</p>	<p>At completion of archeological investigations.</p>	<p>Planning Department/project sponsor</p>	<p>Considered complete after ARR is approved.</p>
<p>Once approved by the ERO, copies of the ARR shall be distributed as follows: California Archeological Site Survey Northwest Information Center (NWIC) shall receive one (1) copy and the ERO shall receive a copy of the transmittal of the ARR to the NWIC. The environmental planning division of the planning department shall receive one (1) bound hardcopy of the ARR. Digital files that shall be submitted to the environmental division include an unlocked, searchable PDF version of the ARR, GIS shapefiles of the site and feature locations, any formal site recordation forms (CA DPR 523 series), and/or documentation for nomination to</p>				

MONITORING AND REPORTING PROGRAM¹

	Implementation Responsibility	Mitigation Schedule	Monitoring / Reporting Responsibility	Monitoring Actions / Completion Criteria
<p>the National Register of Historic Places/California Register of Historical Resources. The PDF ARR, GIS files, recordation forms, and/or nomination documentation should be submitted via USB or other stable storage device. If a descendant group was consulted during archeological treatment, a PDF of the ARR shall be provided to the representative of the descendant group.</p>	<p>Project archeologist prepares collection for curation and project sponsor pays for curation costs.</p>	<p>Upon acceptance by the ERO of the final report.</p>	<p>Planning Department/project sponsor</p>	<p>Upon submittal of the collection for curation the sponsor or archaeologist shall provide a copy of the signed curatorial agreement to the ERO. Considered complete upon acceptance of the collection by the curatorial facility.</p>
<p align="center">TRIBAL CULTURAL RESOURCES</p>				
<p align="center">Project Mitigation Measure 4 (Implementation of Central SoMa PEIR Mitigation Measure M-CP-5): Tribal Cultural Resources Program</p>				
<p><u>Preservation in place.</u> In the event of the discovery of an archeological resource of Native American origin, the Environmental Review Officer (ERO), the project sponsor, and the tribal representative, shall consult to determine whether preservation in place would be feasible and effective. If it is determined that preservation-in-place of the tribal cultural resource would be both feasible and effective, then the archeological consultant shall prepare an archeological resource preservation plan, which shall be implemented by the project sponsor during construction. The consultant shall submit a draft ARPP to Planning for review and approval.</p>	<p>Project sponsor archeological consultant, and ERO, in consultation with the affiliated Native American tribal representatives.</p>	<p>If significant archeological resource is present, during implementation of the project.</p>	<p>Planning Department/project sponsor</p>	<p>Considered complete upon completion and approval of ARPP and project redesign.</p>
<p><u>Interpretive Program.</u> If the ERO, in consultation with the affiliated Native American tribal representatives and the project sponsor, determines that preservation-in-place of the tribal cultural resources is not a sufficient or feasible option, then archeological data recovery shall be implemented as required by the ERO and in consultation with affiliated Native American tribal representatives. In</p>	<p>Project sponsor in consultation with the tribal representative.</p>	<p>After determination that preservation in place is not feasible, and subsequent to archeological data recovery.</p>	<p>Planning Department/project sponsor</p>	<p>Sponsor or archeological consultant shall submit the Tribal Cultural Resources Interpretation Plan to the ERO for review and approval.</p>

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Adopted Mitigation Measures	Implementation Responsibility	Mitigation Schedule	Monitoring / Reporting Responsibility	Monitoring Actions / Completion Criteria
<p>addition, the project sponsor shall implement an interpretive program of the tribal cultural resource in consultation with affiliated tribal representatives. A Tribal Cultural Resources Interpretation Plan produced in consultation with the ERO and affiliated tribal representatives, at a minimum, and approved by the ERO would be required to guide the interpretive program. The plan shall identify, as appropriate, proposed locations for installations or displays, the proposed content and materials of those displays or installation, the producers or artists of the displays or installation, and a long-term maintenance program. The interpretive program may include artist installations, preferably by local Native American artists, oral histories with local Native Americans, cultural displays and interpretation, and educational panels or other informational displays. Upon approval by the ERO and affiliated Native American tribal representatives, and prior to project occupancy, the interpretive program shall be implemented by the project sponsor.</p>				<p>Complete upon sponsor verification to ERO that interpretive program was implemented.</p>

TRANSPORTATION AND CIRCULATION

Project Mitigation Measure 5 (Implementation of Central SoMa PEIR Mitigation Measure M-TR-3a): Transit Enhancements	Project sponsor	Ongoing during project operations	Planning Department and project sponsor.	Abatement of queues during operations
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The project sponsor shall ensure that recurring vehicle queues do not substantially affect public transit operations on the public right-of-way near the off-street vehicular parking facility located on Bryant Street. A vehicle queue is defined as one or more vehicles (destined to the parking facility) blocking any portion of any public street, alley or sidewalk for a consecutive period of three minutes or longer on a daily or weekly basis. If a recurring queue occurs, the owner/operator of the parking facility shall employ abatement methods as needed to abate the queue. Appropriate abatement methods will vary depending on the characteristics and causes of the recurring queue, as well as the characteristics of the parking facility, the street(s) to which the facility connects, and the associated land uses (if applicable). Suggested abatement methods include but are not limited to the following: redesign of facility to improve vehicle circulation and/or onsite queue capacity; employment of parking attendants; installation of LOT FULL signs with active management by parking attendants; use of valet parking or other space-efficient parking techniques; use of off-site parking facilities or shared parking with nearby uses; use of parking occupancy sensors and signage directing drivers to available spaces; transportation demand

MONITORING AND REPORTING PROGRAM¹

	Implementation Responsibility	Mitigation Schedule	Monitoring / Reporting Responsibility	Monitoring Actions / Completion Criteria
<p>management strategies such as those listed in the San Francisco Planning Code Transportation Demand Management Program.</p>				
<p>If the Planning Director, or his or her designee, suspects that a recurring queue is present, the Department shall notify the project sponsor in writing. Upon request, the project sponsor shall hire a qualified transportation consultant to evaluate the conditions at the site for no less than seven days. The consultant shall prepare a monitoring report to be submitted to the Department for review. If the Department determines that a recurring queue does exist, the facility project sponsor shall have 90 days from the date of the written determination to abate the queue.</p>	<p>Project sponsor</p>	<p>Prior to the start of project construction and throughout the construction period.</p>	<p>Project sponsor to develop the plan; San Francisco Municipal Transportation Agency, San Francisco Public Works, and planning department to review and approve.</p>	<p>Considered complete upon approval of construction management plan and completion of project's construction.</p>
<p>Project Mitigation Measure 6 (Implementation of Central SoMa PEIR Mitigation Measure M-TR-9): Construction Management Plan <i>Construction Management Plan</i>—The project sponsor shall develop and, upon review and approval by the SFMTA and Public Works, implement a Construction Management Plan, addressing transportation-related circulation, access, staging and hours of delivery. The Construction Management Plan would disseminate appropriate information to contractors and affected agencies with respect to coordinating construction activities to minimize overall disruption and ensure that overall circulation in the project area is maintained to the extent possible, with particular focus on ensuring transit, pedestrian, and bicycle connectivity. The Construction Management Plan would supplement and expand, rather than modify or supersede, any manual, regulations, or provisions set forth by the SFMTA, Public Works, or other City departments and agencies, and the California Department of Transportation.</p>				
<p>If construction of the proposed project is determined to overlap with nearby adjacent project(s) as to result in transportation-related impacts, the project sponsor or its contractor(s) shall consult with various City departments such as the SFMTA and Public Works, and other interdepartmental meetings as deemed necessary by the SFMTA, Public Works, and the Planning Department, to develop a Coordinated Construction Management Plan. The Coordinated Construction Management Plan, to be prepared by the contractor, would be reviewed by the SFMTA and would address issues of circulation (traffic, pedestrians, and bicycle), safety, parking and other project construction in the area. Based on review of the construction logistics plan, the project may be required to consult with SFMTA Muni Operations prior to construction to review potential effects to nearby transit operations.</p>				

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	Implementation Responsibility	Mitigation Schedule	Monitoring / Reporting Responsibility	Monitoring Actions / Completion Criteria
<p>The Construction Management Plan and, if required, the Coordinated Construction Management Plan, shall include, but not be limited to, the following:</p> <ul style="list-style-type: none"> • <i>Restricted Construction Truck Access Hours</i>—Limit construction truck movements during the hours between 7:00 and 9:00 a.m. and between 4:00 and 7:00 p.m., and other times if required by the SFMTA, to minimize disruption to vehicular traffic, including transit during the a.m. and p.m. peak periods. • <i>Construction Truck Routing Plans</i>—Identify optimal truck routes between the regional facilities and the project site, taking into consideration truck routes of other development projects and any construction activities affecting the roadway network. • <i>Coordination of Temporary Lane and Sidewalk Closures</i>—The project sponsor shall coordinate travel lane closures with other projects requesting concurrent lane and sidewalk closures through interdepartmental meetings, to minimize the extent and duration of requested lane and sidewalk closures. Travel lane closures shall be minimized especially along transit and bicycle routes, so as to limit the impacts to transit service and bicycle circulation and safety. • <i>Maintenance of Transit, Vehicle, Bicycle, and Pedestrian Access</i>—The project sponsor/construction contractor(s) shall meet with Public Works, SFMTA, the Fire Department, Muni Operations and other City agencies to coordinate feasible measures to include in the Coordinated Construction Management Plan to maintain access for transit, vehicles, bicycles and pedestrians. This shall include an assessment of the need for temporary transit stop relocations or other measures to reduce potential traffic, bicycle, and transit disruption and pedestrian circulation effects during construction of the project. • <i>Carpool, Bicycle, Walk and Transit Access for Construction Workers</i>—The construction contractor shall include methods to encourage carpooling, bicycling, walk and transit access to the project site by construction workers (such as providing transit subsidies to construction workers, providing secure bicycle parking spaces, participating in free-to-employee ride matching program from www.511.org, participating in emergency ride home program through the City of San Francisco (www.sferh.org), and providing transit information to construction workers). • <i>Construction Worker Parking Plan</i>—The location of construction worker parking shall be identified as well as the person(s) responsible for monitoring the implementation of the proposed parking plan. The use of on-street parking to accommodate construction worker parking shall 				

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	Implementation Responsibility	Mitigation Schedule	Monitoring / Reporting Responsibility	Monitoring Actions / Completion Criteria
<p>be discouraged. All construction bid documents shall include a requirement for the construction contractor to identify the proposed location of construction worker parking. If on-site, the location, number of parking spaces, and area where vehicles would enter and exit the site shall be required. If off-site parking is proposed to accommodate construction workers, the location of the off-site facility, number of parking spaces retained, and description of how workers would travel between the off-site facility and project site shall be required.</p> <ul style="list-style-type: none"> • <i>Project Construction Updates for Adjacent Businesses and Residents</i>—To minimize construction impacts on access for nearby institutions and businesses, the project sponsor shall provide nearby residences and adjacent businesses with regularly updated information regarding project construction, including construction activities, peak construction vehicle activities (e.g., concrete pours), travel lane closures, and lane closures. At regular intervals to be defined in the Construction Management Plan and, if necessary, in the Coordinated Construction Management Plan, a regular email notice shall be distributed by the project sponsor that shall provide current construction information of interest to neighbors, as well as contact information for specific construction inquiries or concerns. 				
<p>NOISE</p>				
<p>Project Mitigation Measure 8 (Implementation of Central SoMa PEIR Mitigation Measure M-NO-2a): General Construction Noise Control Measures</p>				
<p>The project sponsor shall undertake the following:</p> <ul style="list-style-type: none"> • Require the general contractor to ensure that equipment and trucks used for project construction utilize the best available noise control techniques (e.g., improved mufflers, equipment redesign, use of intake silencers, ducts, engine enclosures and acoustically-attenuating shields or shrouds), wherever feasible. • Require the general contractor to locate stationary noise sources (such as compressors) as far from adjacent or nearby sensitive receptors as possible, to muffle such noise sources, and to construct barriers around such sources and/or the construction site, which could reduce construction noise by as much as 5 dBA. To further reduce noise, the contractor shall locate stationary equipment in pit areas or excavated areas, if feasible. • Require the general contractor to use impact tools (e.g., jack hammers, pavement breakers, and rock drills) that are hydraulically or electrically 	<p>Project sponsor</p>	<p>Prior to the issuance of building permits and along with the submission of construction documents to the planning department for development of the noise control plan. Project sponsor to implement the noise control plan during construction. If noise monitoring is required, reporting to be submitted to the planning</p>	<p>Planning department to review and approve the noise control plan and to review monitoring reports, as needed; health department or police department for noise complaints.</p>	<p>Considered complete upon approval of the Construction Noise Control Plan, submittal of any required monitoring reports and after construction is complete.</p>

MONITORING AND REPORTING PROGRAM¹

	Implementation Responsibility	Mitigation Schedule	Monitoring / Reporting Responsibility	Monitoring Actions / Completion Criteria
<p>powered whenever possible to avoid noise associated with compressed air exhaust from pneumatically powered tools. Where use of pneumatic tools is unavoidable, an exhaust muffler on the compressed air exhaust shall be used, along with external noise jackets on the tools, which could reduce noise levels by as much as 10 dBA.</p> <ul style="list-style-type: none"> • Include noise control requirements in specifications provided to construction contractors. Such requirements could include, but are not limited to, performing all work in a manner that minimizes noise to the extent feasible; use of equipment with effective mufflers; undertaking the most noisy activities during times of least disturbance to surrounding residents and occupants, as feasible; and selecting haul routes that avoid residential buildings to the extent that such routes are otherwise feasible. <p>Prior to the issuance of a building permit, along with the submission of construction documents, submit to the Planning Department and Department of Building Inspection (DBI) a list of measures that shall be implemented and that shall respond to and track complaints pertaining to construction noise (noise control plan). These measures shall include (1) a procedure and phone numbers for notifying DBI and the Police Department (during regular construction hours and off-hours); (2) a sign posted onsite describing noise complaint procedures and a complaint hotline number that shall be answered at all times during construction; (3) designation of an on-site construction complaint and enforcement manager for the project; and (4) notification of neighboring residents and non-residential building managers within 300 feet of the project construction area at least 30 days in advance of extreme noise generating activities (defined as activities generating anticipated noise levels of 80 dBA or greater without noise controls, which is the standard in the Police Code) about the estimated duration of the activity.</p>		<p>department regularly as established in the noise control plan.</p>		<p>Project plans indicate proposed generator is Tier 4 interim/final (May 27, 2021 plan set, page A-047). Maintenance portion is ongoing and records are subject to Planning Department review upon request.</p>
<p>AIR QUALITY</p> <p>Project Mitigation Measure 9 (Implementation of Central SoMa PEIR Mitigation M-AQ-5a): Best Available Control Technology for Diesel Generators and Fire Pumps</p> <p>All diesel generators shall have engines that (1) meet Tier 4 Final or Tier 4 Interim emission standards, or (2) meet Tier 2 emission standards and are equipped with a California Air Resources Board Level 3 Verified Diesel Emissions Control Strategy. For each new diesel backup generator permit submitted for the project, including any associated generator pads, engine and filter specifications shall be submitted to the San Francisco Planning Department for review and approval prior to issuance of a permit for the generator from the San Francisco Department</p>	<p>Project sponsor</p>	<p>For specifications, prior to issuance of building permit for diesel generator. For maintenance, ongoing.</p>	<p>Planning department (Environmental Review Officer, Air Quality technical staff) to review and approve.</p>	

MONITORING AND REPORTING PROGRAM¹

	Implementation Responsibility	Mitigation Schedule	Monitoring / Reporting Responsibility	Monitoring Actions / Completion Criteria
<p>of Building Inspection. Once operational, all diesel backup generators and Verified Diesel Emissions Control Strategy shall be maintained in good working order in perpetuity and any future replacement of the diesel backup generators and Level 3 Verified Diesel Emissions Control Strategy filters shall be required to be consistent with these emissions specifications. The operator of the facility shall maintain records of the testing schedule for each diesel backup generator for the life of that diesel backup generator and provide this information for review to the Planning Department within three months of requesting such information.</p>	<p>Project sponsor and construction contractor</p>	<p>Prior to issuance of construction permits project sponsor to submit:</p> <ol style="list-style-type: none"> 1. Construction emissions minimization plan for review and approval, and 2. Signed certification statement 	<p>Planning department (Environmental Review Officer, Air Quality technical staff) to review and approve/ project sponsor to report.</p>	<p>Considered complete upon Planning Department review and acceptance of Construction Emissions Minimization Plan, implementation of the plan, submittal of quarterly and final reports, and completion of project construction activities.</p>
<p>Project Mitigation Measure 10 (Implementation of Central SOMA PEIR Mitigation Measure M-AQ-6a): Construction Emissions Minimization Plan</p> <p>The project sponsor shall submit a Construction Emissions Minimization Plan (Plan) to the Environmental Review Officer (ERO) for review and approval by an Environmental Planning Air Quality Specialist. The Plan shall be designed to reduce air pollutant emissions to the greatest degree practicable.</p>				
<p>The Plan shall detail project compliance with the following requirements:</p>				
<ol style="list-style-type: none"> 1. All off-road equipment greater than 25 horsepower and operating for more than 20 total hours over the entire duration of construction activities shall meet the following requirements: <ol style="list-style-type: none"> a) Where access to alternative sources of power are available, portable diesel engines shall be prohibited; b) All off-road equipment shall have: <ol style="list-style-type: none"> i. Engines that meet or exceed either U.S. Environmental Protection Agency or California Air Resources Board Tier 3 off-road emission standards, and ii. Engines that are retrofitted with an ARB Level 3 Verified Diesel Emissions Control Strategy (VDECS)¹ 				
<ol style="list-style-type: none"> c) Exceptions: <ol style="list-style-type: none"> i. Exceptions to 1(a) may be granted if the project sponsor has submitted information providing evidence to the satisfaction of the ERO that an alternative source of power is limited or infeasible at the project site and that the requirements of this exception provision apply. Under this circumstance, the sponsor shall submit documentation of compliance with 1(b) for onsite power generation. ii. Exceptions to 1(b) 				

¹ Equipment with engines meeting Tier 4 Interim or Tier 4 Final emission standards automatically meet the Tier 3 emissions standard and level 3 VDECS requirement, therefore VDECS would not be required.

MONITORING AND REPORTING PROGRAM¹

Adopted Mitigation Measures

(ii) may be granted if the project sponsor has submitted information providing evidence to the satisfaction of the ERO that a particular piece of off-road equipment with an ARB Level 3 VDECS (1) is technically not feasible, (2) would not produce desired emissions reductions due to expected operating modes, (3) installing the control device would create a safety hazard or impaired visibility for the operator, or (4) there is a compelling emergency need to use off-road equipment that are not retrofitted with an ARB Level 3 VDECS and the sponsor has submitted documentation to the ERO that the requirements of this exception provision apply. If granted an exception to 1(b)(ii), the project sponsor shall comply with the requirements of 1(c)(iii).

iii. If an exception is granted pursuant to 1(c)(ii), the project sponsor shall provide the next-cleanest piece of off-road equipment as provided by the step-down schedule in Table M-AQ-4:

M-AQ-Road	Compliance Alternative	Engine Emission Standard	Emissions Control
	1		Tier 3**
2		Tier 3	ARB Level 1 VDECS

Equipment Compliance Step Down Schedule*

**Table 4
Off-**

* How to use the table. If the requirements of 1(b) cannot be met, then the project sponsor would need to meet Compliance Alternative 1. Should the project sponsor not be able to supply off-road equipment meeting Compliance Alternative 1, then Compliance Alternative 2 would need to be met.

** Minimum Tier 3 off road emissions standards are required because the project exceeds the construction criteria air pollutant screening levels. Tier 3 and Tier 4 equipment substantially reduce NOx and PM emissions.

Implementation Responsibility

Mitigation Schedule

Monitoring / Reporting Responsibility

Monitoring Actions / Completion Criteria

MONITORING AND REPORTING PROGRAM¹

	Adopted Mitigation Measures	Implementation Responsibility	Mitigation Schedule	Monitoring / Reporting Responsibility	Monitoring Actions / Completion Criteria
2.	The project sponsor shall require the idling time for off-road and on-road equipment be limited to no more than two minutes, except as provided in exceptions to the applicable State regulations regarding idling for off-road and on-road equipment. Legible and visible signs shall be posted in multiple languages (English, Spanish, Chinese) in designated queuing areas and at the construction site to remind operators of the two-minute idling limit.				
3.	The project sponsor shall require that construction operators properly maintain and tune equipment in accordance with manufacturer specifications.				
4.	The Plan shall include estimates of the construction timeline by phase with a description of each piece of off-road equipment required for every construction phase. Off-road equipment descriptions and information may include, but is not limited to, equipment type, equipment manufacturer, equipment identification number, engine model year, engine certification (Tier rating), horsepower, engine serial number, and expected fuel usage and hours of operation. For the VDECS installed: technology type, serial number, make, model, manufacturer, ARB verification number level, and installation date and hour meter reading on installation date.				
5.	The Plan shall be kept on-site and available for review by any persons requesting it and a legible sign shall be posted at the perimeter of the construction site indicating to the public the basic requirements of the Plan and a way to request a copy of the Plan. The project sponsor shall provide copies of the Plan as requested.				
6.	<i>Reporting.</i> Quarterly reports shall be submitted to the ERO indicating the construction phase and off-road equipment information used during each phase including the information required in Paragraph 4, above. Within six months of the completion of construction activities, the project sponsor shall submit to the ERO a final report summarizing construction activities. The final report shall indicate the start and end dates and duration of each construction phase. For each phase, the report shall include detailed information required in Paragraph 4.				
7.	<i>Certification Statement and On-site Requirements.</i> Prior to the commencement of construction activities, the project sponsor shall certify (1) compliance with the Plan, and (2) all applicable requirements of the Plan have been incorporated into contract specifications.				

WIND

MONITORING AND REPORTING PROGRAM¹

	Adopted Mitigation Measures	Implementation Responsibility	Mitigation Schedule	Monitoring / Reporting Responsibility	Monitoring Actions / Completion Criteria
	<p>Project Mitigation Measure 12 (Implementation of Central SoMa PEIR Mitigation Measure M-WI-1): Wind Hazard Criterion for Plan Area</p> <p>The project sponsor shall maintain all wind reduction features (location, minimum height and minimum canopy) shown on the May 27, 2021 plan set (pages A-043 and L-01) required to mitigate wind hazard exceedances for the life of the project.</p>	Project sponsor	Ongoing after installation of landscaping features.	Planning Department/ project sponsor	Ongoing during project operations. Project sponsor to submit documentation to the Planning Department upon request.

BIOLOGICAL RESOURCES

	<p>Project Mitigation Measure 13 (Implementation of Central SoMa PEIR Mitigation Measure M-BI-1): Pre-Construction Bat Surveys</p> <p>The project sponsor shall conduct preconstruction special-status bat surveys for the removal of trees with a diameter at breast height equal to or greater than 6 inches. If active day or night roosts are found, a qualified biologist (i.e., a biologist holding a California Department of Fish and Wildlife (CDFW) collection permit and a Memorandum of Understanding with the CDFW allowing the biologist to handle and collect bats) shall take actions to make such roosts unsuitable habitat prior to tree removal. A no disturbance buffer shall be created around active bat roosts being used for maternity or hibernation purposes at a distance to be determined in consultation with CDFW. Bat roosts initiated during construction are presumed to be unaffected, and no buffer would be necessary.</p>	Project sponsor	Prior to issuance of building permits project sponsor to provide scope of work for bat surveys for review and approval by the planning department.	Planning department to review and approve bat survey scope of work.	Considered complete upon issuance of building permits and completion of bat survey work conducted in accordance with the approved scope of work.
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IMPROVEMENT MEASURES AGREED TO BY PROJECT SPONSOR

	<p>Project Improvement Measure 1 (Implementation of Central SoMa PEIR Improvement Measure I-BI-2): Night Lighting Minimization</p> <p>The project sponsor should implement bird-safe building operations to prevent and minimize bird strike impacts, including but not limited to the following measures:</p> <ul style="list-style-type: none"> • Reduce building lighting from exterior sources by: <ul style="list-style-type: none"> • Minimizing the amount and visual impact of perimeter lighting and façade up-lighting and avoid up-lighting of rooftop antennae and other tall equipment, as well as of any decorative features; • Installing motion-sensor lighting; • Utilizing minimum wattage fixtures to achieve required lighting levels. 	Project sponsor	Ongoing during project operation	Planning Department/ project sponsor	Considered complete upon approval of building plans by Planning Department. Planning Department may engage in follow up discussions with project sponsor, as applicable.
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MONITORING AND REPORTING PROGRAM¹

	Implementation Responsibility	Mitigation Schedule	Monitoring / Reporting Responsibility	Monitoring Actions / Completion Criteria
<ul style="list-style-type: none"> • Reduce building lighting from interior sources by: <ul style="list-style-type: none"> • Dimming lights in lobbies, perimeter circulation areas, and atria; • Turning off all unnecessary lighting by 11 p.m. through sunrise, especially during peak migration periods (mid-March to early June and late August through late October); • Utilizing automatic controls (motion sensors, photo-sensors, etc.) to shut off lights in the evening when no one is present; • Encouraging the use of localized task lighting to reduce the need for more extensive overhead lighting; • Scheduling nightly maintenance to conclude by 11 p.m.; • Educating building users about the dangers of night lighting to birds 				

¹ *Definitions of MMRP Column Headings:*

Adopted Mitigation Measures: Full text of the mitigation measure(s).

Implementation Responsibility: Entity who is responsible for implementing the mitigation measure. In most cases this is the project sponsor and/or project's sponsor's contractor/consultant and at times under the direction of the planning department.

Mitigation Schedule: Identifies milestones for when the actions in the mitigation measure need to be implemented.

Monitoring/Reporting Responsibility: Identifies who is responsible for monitoring compliance with the mitigation measure and any reporting responsibilities. In most cases it is the Planning Department who is responsible for monitoring compliance with the mitigation measure. If a department or agency other than the planning department is identified as responsible for monitoring, there should be an expressed agreement between the planning department and that other department/agency. In most cases the project sponsor, their contractor, or consultant are responsible for any reporting requirements.

Monitoring Actions/Completion Criteria: Identifies the milestone at which the mitigation measure is considered complete. This may also identify requirements for verifying compliance.



555-585 Bryant Street

San Francisco, CA 94107

Housing Sustainability District Application Drawings

05 — 27 — 2021

SHEET #	NAME				
G-000	COVER SHEET	A-040	BASEMENT LEVEL -3 PLAN - PROPOSED	C-080	SDAT REVIEW LETTER 1 PG 1-2
G-001	DRAWING INDEX	A-041	BASEMENT LEVEL -2 PLAN - PROPOSED	C-081	SDAT REVIEW LETTER 1 PG 3-4
G-001A	OVERALL HSD CHANGES NARRATIVE	A-042	BASEMENT LEVEL -1 PLAN - PROPOSED	C-082	SDAT REVIEW LETTER 1 PG 5
G-002	AERIAL SITE VIEW	A-043	GROUND FLOOR PLAN - PROPOSED	C-090	PLANNING CODE REVIEW CHECKLIST March 10, 2021
G-003	SITE LOCATION	A-044	SECOND FLOOR PLAN - PROPOSED	C-091	PLANNING CODE REVIEW CHECKLIST March 10, 2021
G-004	SURVEY SHEET	A-045	TYPICAL FLOOR PLAN - PROPOSED	C-092	PLANNING CODE REVIEW CHECKLIST March 10, 2021
G-005	STREET VIEW - BRYANT STREET (EXISTING)	A-046	SIXTEENTH FLOOR PLAN - PROPOSED	C-093	PLANNING CODE REVIEW CHECKLIST March 10, 2021
G-006	STREET VIEW - WELSH STREET (EXISTING)	A-047	ROOF PLAN - PROPOSED	C-094	URBAN DESIGN GUIDELINES MATRIX PAGE 1-2
G-007	SITE PLAN - EXISTING	A-048	BRYANT STREET ELEVATION - PROPOSED	C-095	PLANNING CODE REVIEW CHECKLIST March 14, 2021
G-008	STREET ELEVATIONS - EXISTING	A-049	WELSH STREET ELEVATION - PROPOSED	C-096	PLANNING CODE REVIEW CHECKLIST March 14, 2021
G-009	SITE SECTIONS - EXISTING	A-050	SIDE VIEW ELEVATIONS - PROPOSED	C-097	PLANNING CODE REVIEW CHECKLIST March 14, 2021
G-010	NEIGHBORHOOD W/ PENDING PROJECTS	A-051	MATERIAL PALETTE - PROPOSED	C-098	SDAT REVIEW LETTER 2 PG 1-2
		A-052	MATERIALS BOARD - PROPOSED	C-099	SDAT REVIEW LETTER 2 PG 3-4
		A-053	BUILDING SECTION - PROPOSED	A-100	BASEMENT EGRESS PLAN B2 - PROPOSED
		A-054	BUILDING SECTION - PROPOSED	A-101	BASEMENT EGRESS PLAN B1 - PROPOSED
		A-055	PROJECT DATA - PROPOSED	A-102	GROUND FLOOR EGRESS PLAN - PROPOSED
A-011	PROJECT DESCRIPTION	L-01	GROUND FLOOR STREETSCAPE PLAN	A-103	SECOND FLOOR EGRESS PLAN - PROPOSED
A-012	ZONING ENVELOPE - BASELINE	L-02	BRYANT STREET SECTIONS	A-104	TYPICAL FLOOR EGRESS PLAN - PROPOSED
A-013	BUILDING SECTION - BASELINE	L-03	WELSH STREET SECTIONS	A-105	SIXTEENTH FLOOR EGRESS PLAN - PROPOSED
A-014	BUILDING SECTION - BASELINE	L-04	COURTYARD LANDSCAPE PLAN	A-106	LOWER ROOF EGRESS PLAN - PROPOSED
A-015	PROJECT DATA - BASELINE	L-05	ROOF TERRACE LANDSCAPE PLAN		
A-016	GROUND FLOOR PLAN - BASELINE				
A-017	TYPICAL FLOOR PLAN - BASELINE				
	PROPOSED PROJECT		APPENDIX		DRIVEWAY AND LOADING OPERATION PLAN
A-020	ZONING INFORMATION	A-060	DESIGN GUIDELINES RESPONSES		TRANSPORT DEMAND MANAGEMENT PLAN
A-021	ZONING ENVELOPE - PROPOSED	A-061	DESIGN GUIDELINES RESPONSES		
A-022	SITE ANALYSIS	A-062	DESIGN GUIDELINES RESPONSES		
A-023	MASSING AND BULK DIAGRAMS	A-063	DESIGN GUIDELINES RESPONSES		
A-024	EXISTING RIGHT-OF-WAY DIAGRAM	A-064	PLANNING REVIEW RESPONSE EXHIBITS		
		A-065	PLANNING REVIEW RESPONSE EXHIBITS		
		A-066	PLANNING REVIEW RESPONSE EXHIBITS		
		A-067	PLANNING REVIEW RESPONSE EXHIBITS		
		A-068	PLANNING REVIEW RESPONSE EXHIBITS		
		A-070	BASEMENT PLAN B2 - ALTERNATE		
		A-071	BASEMENT PLAN B1 - ALTERNATE		
		A-072	GROUND FLOOR PLAN - ALTERNATE		

555-585 BRYANT – OVERALL HSD CHANGES NARRATIVE:

The project sponsor has received review comments from the Planning Department and Street Design Advisory Team and has incorporated revisions to the project design as described below. The Planning Code Review, SDAT Review Letter, and Urban Design Guidelines Matrix are included for reference in the Appendix of this submittal document.

In the State Density Base project portion of the submittal the Project Sponsor has eliminated the basement, adjusted the ground floor to include all of the required PDR space with the use of a mezzanine, and adjusted the residential floor to floor height to 9'-0" as requested. The area tabulation for the Base project has been updated to reflect these changes.

The Proposed project has modified the list of waiver and incentive requests. The request to provide less open space than required by code is now listed as an incentive. The request to provide less than 50% of the roof area as Living Roof is now listed as a ZA waiver with additional information provided on page A-066. An additional ZA waiver has been requested to eliminate the required Off Street Freight Loading space for the PDR space since the intended Trade Shop use is unlikely to have enough demand to warrant the additional space. The previously requested waiver for showers and lockers at the PDR space has been eliminated.

The Project drawings have been revised to reflect responses to the comments from Planning and SDAT. At the ground floor additional notations have been added to clarify a number of items at the streetscape and building interior including but not limited to dimensions, use, parking, and loading space labels. See G-004 Survey, A-043, and A-056 proposed plan drawings.

Per SDAT supported Option 2 from item 2 in SDAT Review Letter 1 the Project proposes to widen the sidewalk at Welsh to 14ft total and remove street parking along the project's frontage while ensuring 14ft clear width for fire and emergency access along Welsh Street.

Along the Welsh Street elevation the use of masonry cladding has been increased at the base of the building adding to its apparent 'heft' while preserving the separation between the traditional masonry expression of the base and the highly articulated and geometric angled bay language of the upper portions of the building. Enlarged rendering exhibits on page C-067 show how windows will be recessed from the face of adjacent facades to provide additional depth and visual texture.

The vehicle entry door at Bryant Street has been reduced to 20ft in width, and one of the required Residential off-street loading spaces has been replaced with 2 service vehicle spaces at Basement 2 per 153(a)(6). In conjunction with this change the required clear height for the single remaining full size loading space is reduced to 12ft as allowed per 154(b)(2). The number of dedicated Car Share parking spaces has been increased to 4 and the required 200 Class I bicycle spaces are now labelled on the plan.

Open space for the project is provided primarily at the south facing roof terrace, private balconies on the south facade, and at the ground floor residential entry stoops at Welsh Street. These locations have been chosen due to the proximity of the site to I-880 and its Bryant Street off-ramp to the north along with the limited solar access to the project's interior courtyards. Two intensively landscaped courtyards at Level 2 contribute to the green roof area for the project and are 30ft wide to allow adequate exposure for dwelling units not facing onto Bryant or Welsh.

In the 'Alternate Scheme' the previously labelled Flex Amenity space at the ground floor facing Bryant Street has been re-designated a "Co-working / Resident Lounge" as part of the overall residential use. This space will be very active during the daytime as a place for residents to both work from home and have social interactions. The 'Alternate Scheme' now includes an additional basement level making it more compatible with the primary proposed scheme. This change does include any increase in requested parking quantity.

The required Driveway and Loading Operations Plan and Transport Demand Management Study are included in the appendix of this submittal. A Wind Study, Shadow Study, and Environmental Noise Study will be submitted separately.

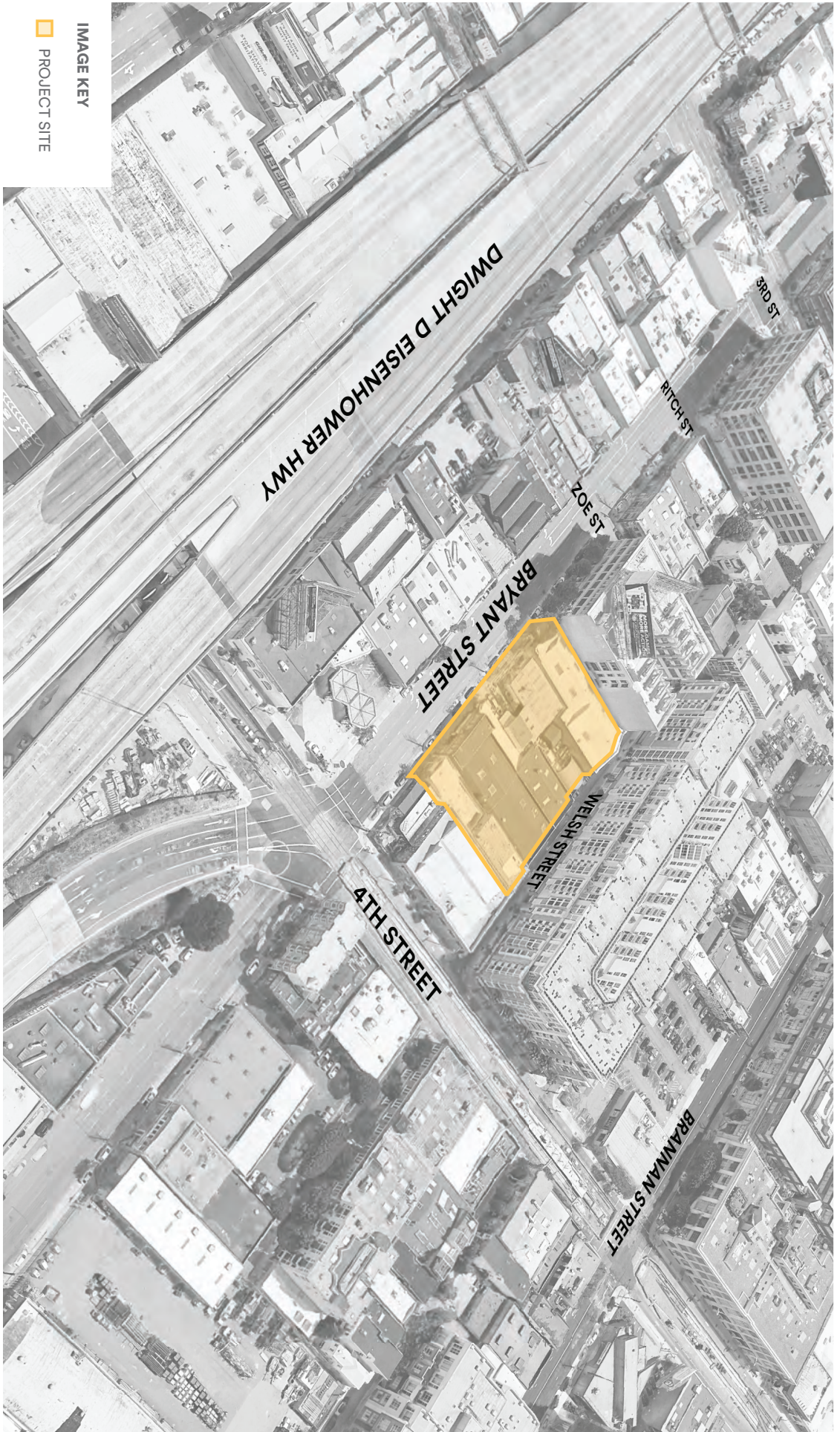


IMAGE KEY

PROJECT SITE

AERIAL SITE VIEW

Housing Sustainability District Application Drawings
555-585 Bryant Street
San Francisco, CA 94107

05-27-2021

G-002

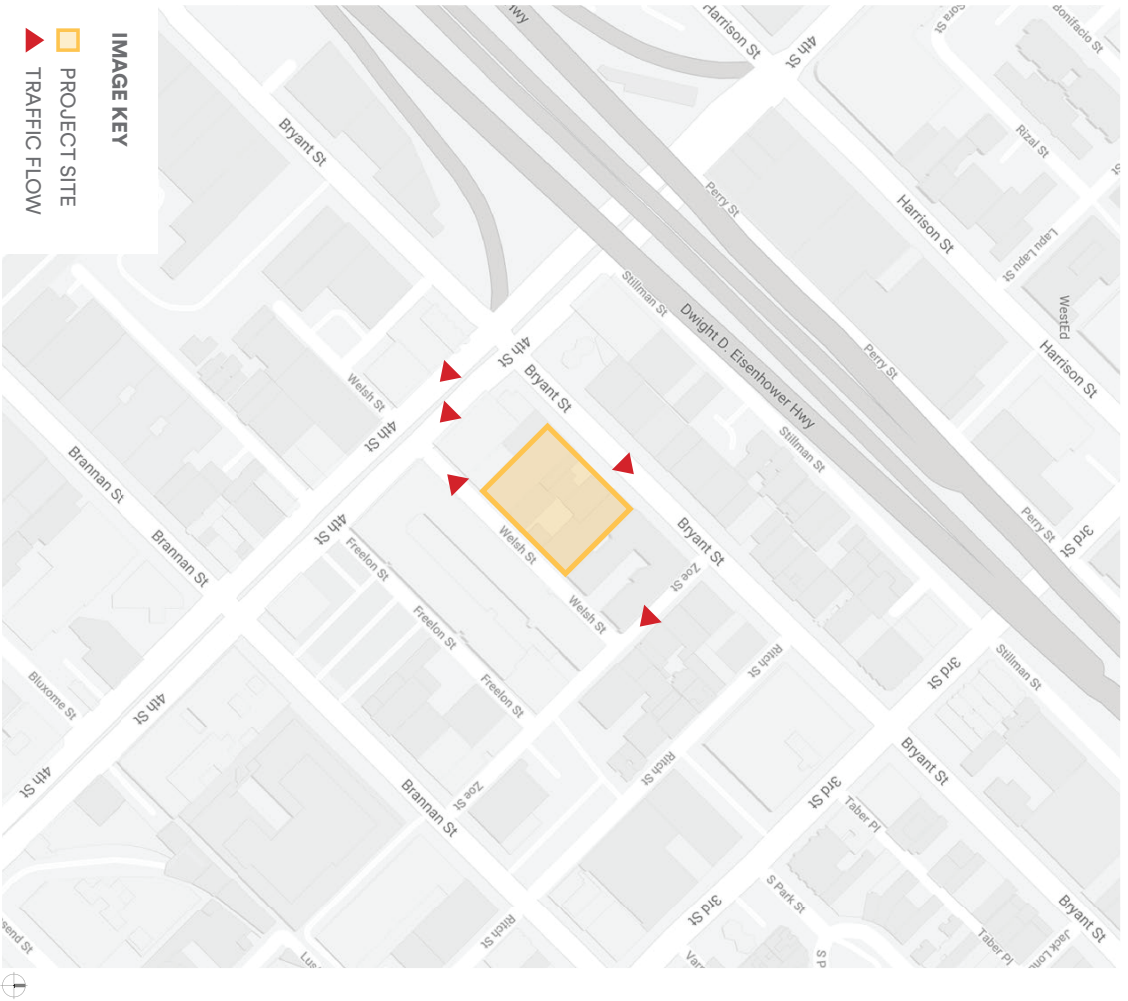
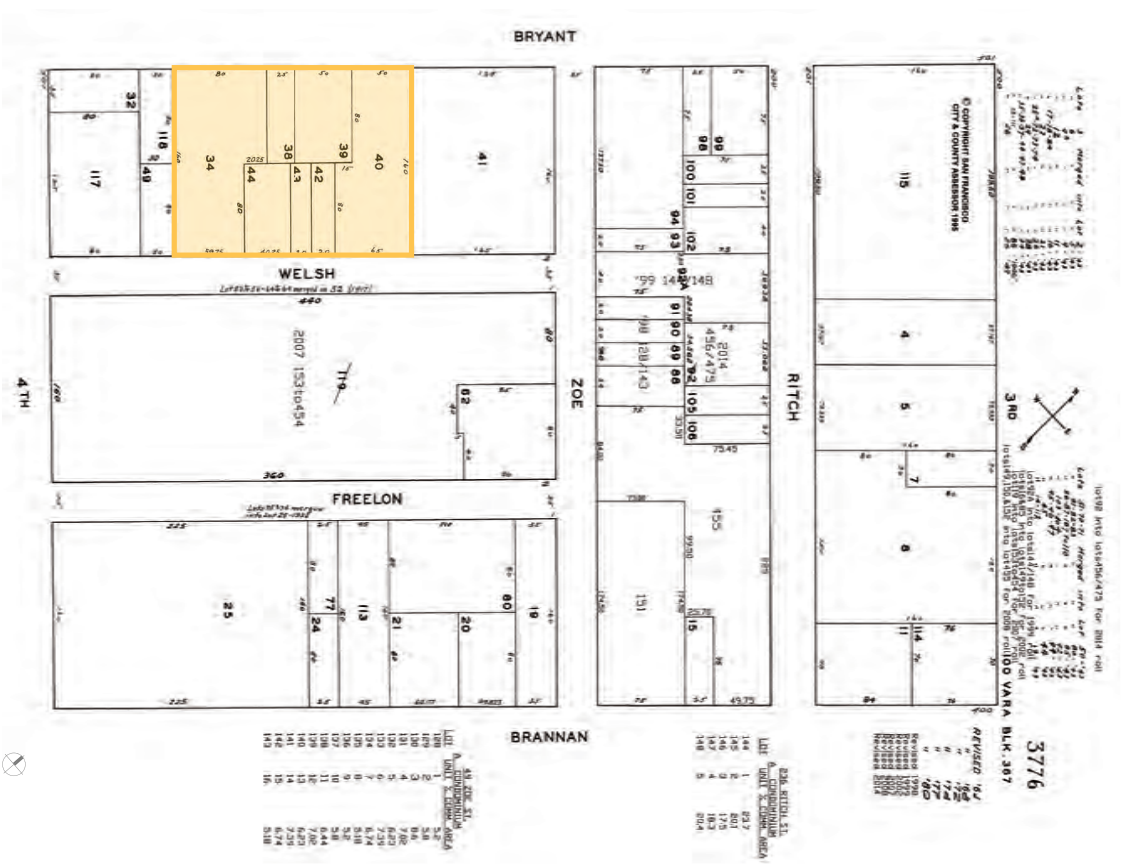


IMAGE KEY
 □ PROJECT SITE
 ▲ TRAFFIC FLOW





NORTH SIDE OF BRYANT - EXISTING CONDITIONS



SOUTH SIDE OF BRYANT - EXISTING CONDITIONS



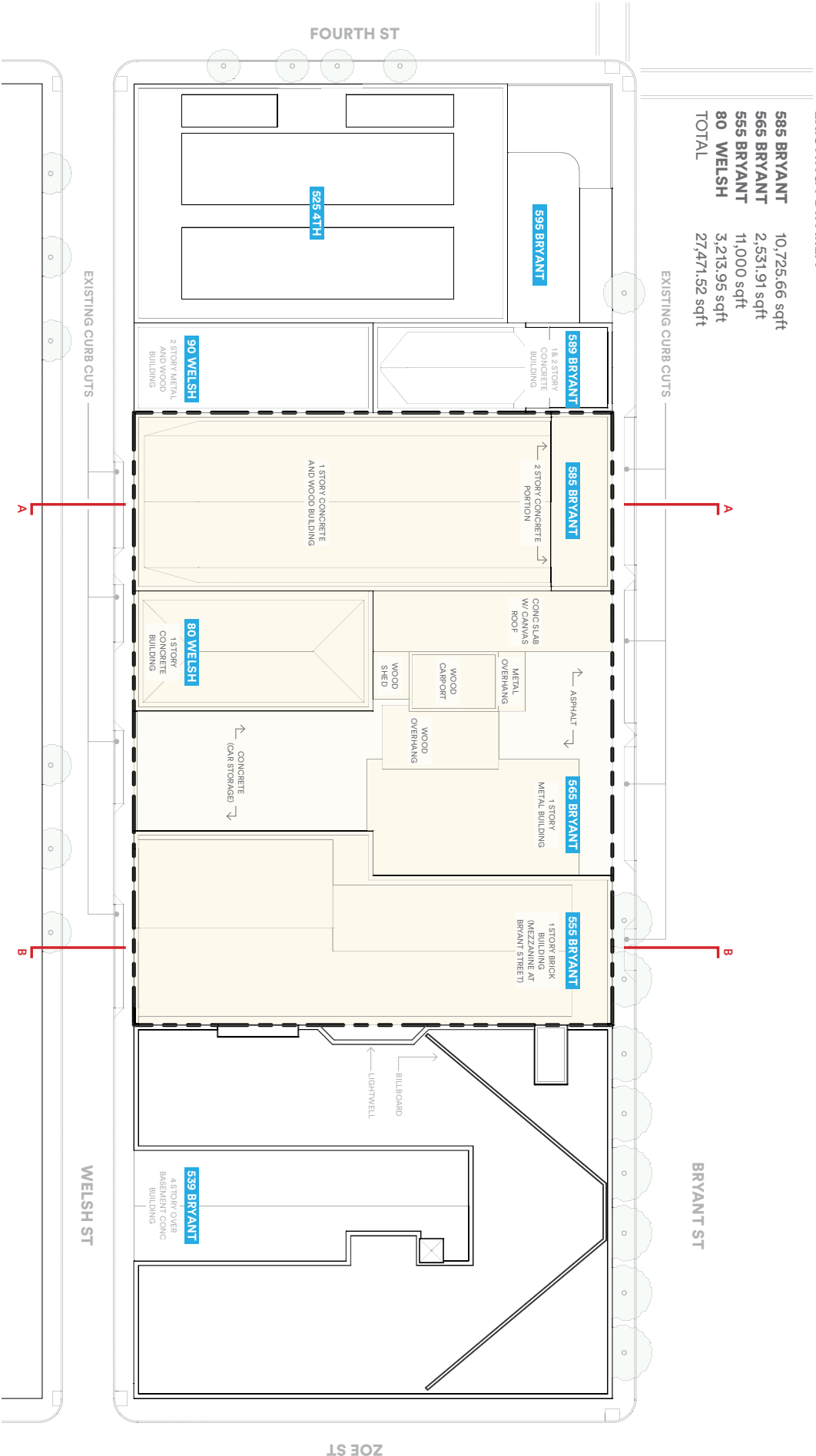
NORTH SIDE OF WELSH - EXISTING CONDITIONS

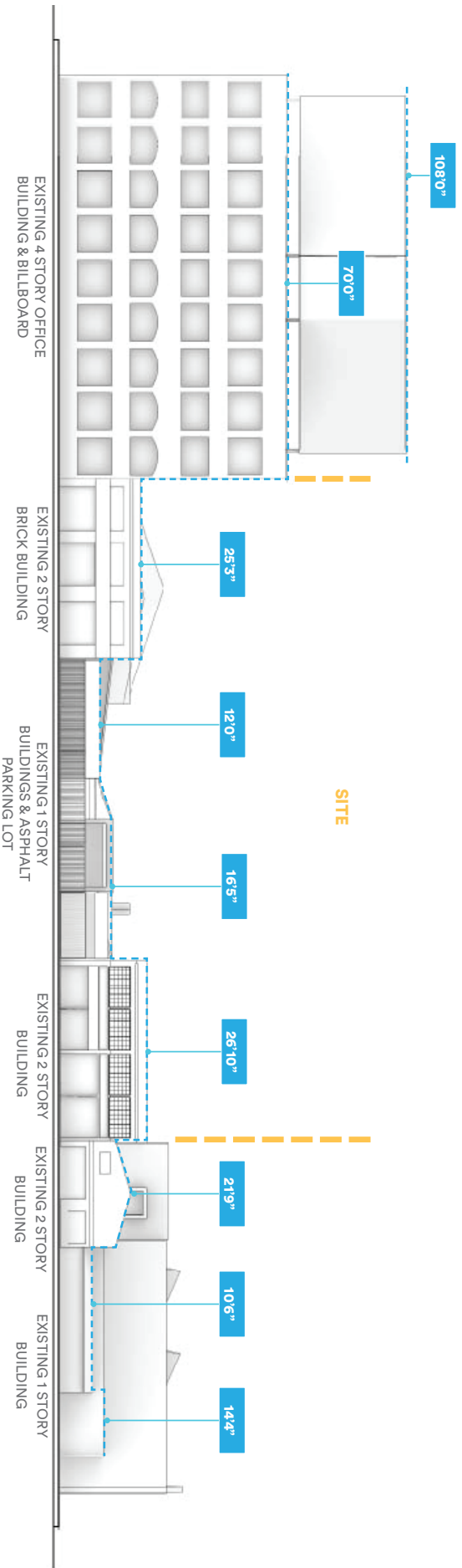


SOUTH SIDE OF WELSH - EXISTING CONDITIONS

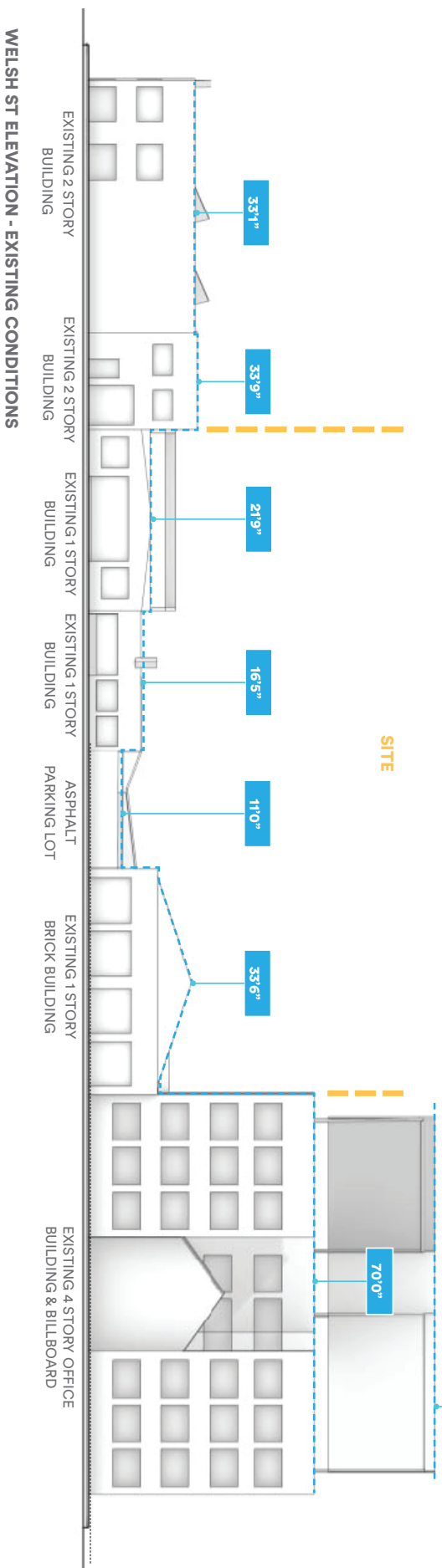
EXISTING PDR AREA

585 BRYANT	10,725.66 sqft
565 BRYANT	2,531.91 sqft
555 BRYANT	11,000 sqft
80 WELSH	3,213.95 sqft
TOTAL	27,471.52 sqft

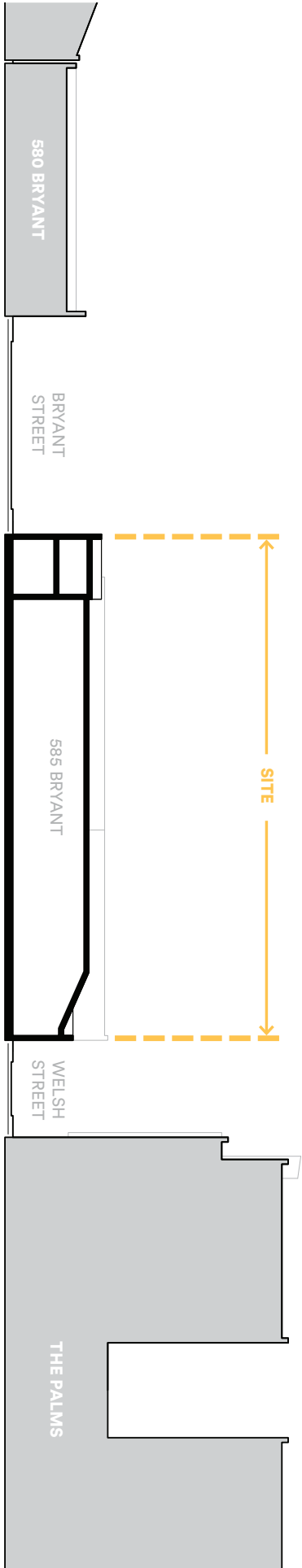




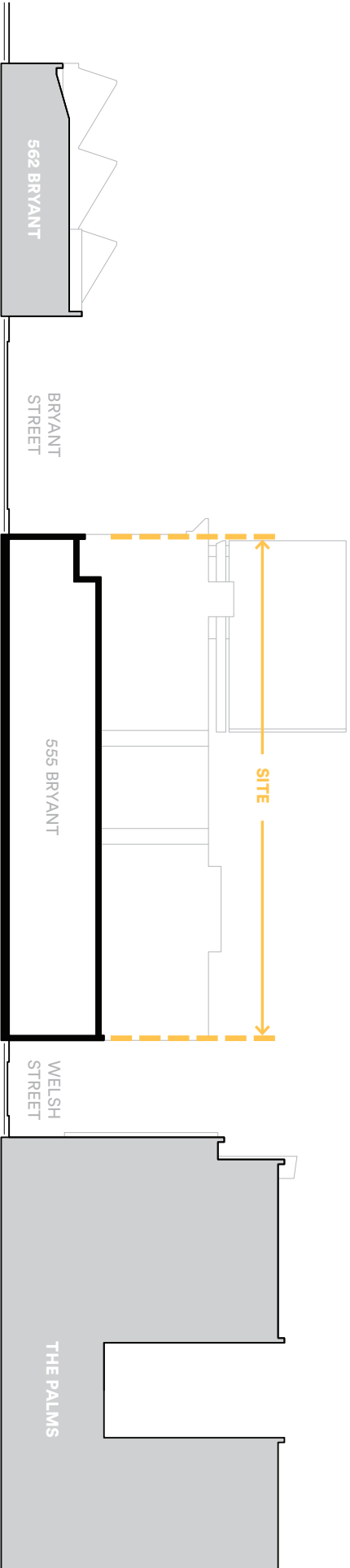
BRYANT ST ELEVATION - EXISTING CONDITIONS



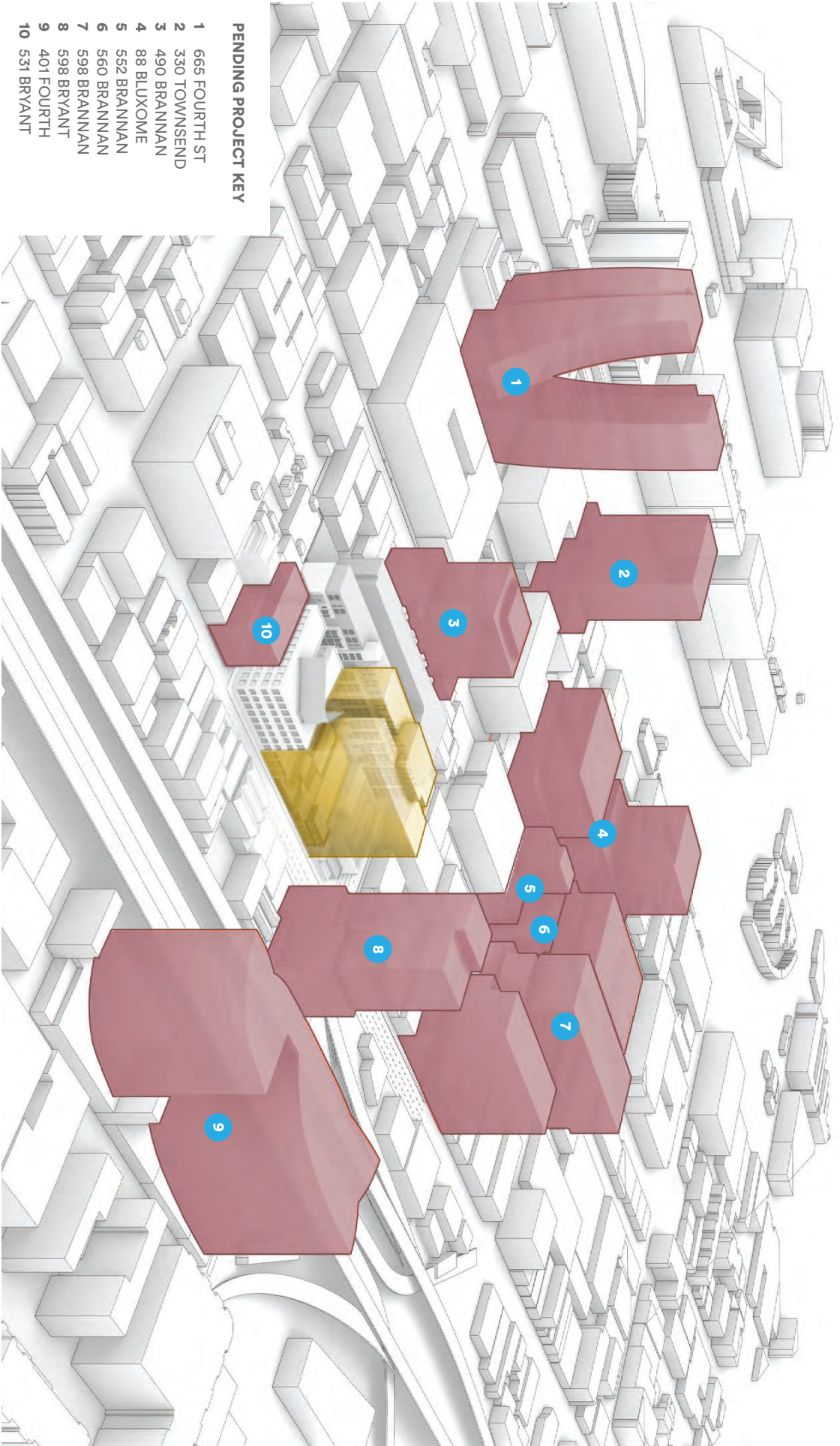
WELSH ST ELEVATION - EXISTING CONDITIONS



SITE SECTION A - EXISTING CONDITIONS



SITE SECTION B - EXISTING CONDITIONS



PENDING PROJECT KEY

- 1 665 FOURTH ST
- 2 330 TOWNSEND
- 3 490 BRANNAN
- 4 88 BLUXOME
- 5 552 BRANNAN
- 6 560 BRANNAN
- 7 598 BRANNAN
- 8 598 BRYANT
- 9 401 FOURTH
- 10 531 BRYANT

State Density Bonus - Baseline Project

PROJECT DESCRIPTION

THE PROPOSED PROJECT COMBINES 7 LOTS BETWEEN BRYANT AND WELSH STREETS MID-BLOCK BETWEEN 4TH STREET AND ZOE STREET IN THE CENTRAL SOMA DISTRICT. CURRENT PDR BUILDINGS WILL BE REPLACED WITH A 160 FT TALL MIXED USE MULTI-FAMILY RESIDENTIAL BUILDING INCORPORATING 500 DWELLING UNITS, 115 ACCESSORY PARKING SPACES, AND 20,604 SF OF PDR USE SPACE. THE PROJECT IS SEEKING EXPEDITED APPROVAL PURSUANT TO THE CENTRAL SOMA HOUSING SUSTAINABILITY DISTRICT. THE PROJECT WILL USE THE STATE DENSITY BONUS LAW MECHANISM TO INCREASE THE SIZE OF THE BUILDING ALLOWED UNDER CURRENT CMUO ZONING BUT WILL NOT EXCEED 160 FT IN HEIGHT SO THAT IT QUALIFIES FOR THE HOUSING SUSTAINABILITY DISTRICT.

THE EXHIBITS IN THIS DRAWING PACKAGE INCLUDE A STATE DENSITY BONUS 'BASELINE' DESIGN SHOWING HOW MUCH RESIDENTIAL AREA THE SITE COULD YIELD WHILE COMPLYING WITH THE PLANNING CODE WITHOUT WAIVERS OR OTHER EXEMPTIONS. THE PROPOSED PROJECT IS LIMITED BY HOUSING SUSTAINABILITY DISTRICT RULES TO 160 FT IN HEIGHT AND THEREFORE TO MAXIMIZE THE AMOUNT OF HOUSING THE PROJECT CAN PROVIDE THE PROJECT REQUESTS SEVERAL WAIVERS AND INCENTIVES:

REQUESTED WAIVERS:

- SETBACKS PER 132.4
- BAY WINDOW DIMENSION CONTROLS PER 136 AND 307(H)(1)(F)
- MINIMUM FLOOR TO FLOOR HEIGHT FOR PDR USES PER 145.(C)(4) & 249.78(D)(10)
- RESIDENTIAL OPEN SPACE PER 140
- NUMBER OF OFF STREET FREIGHT LOADING SPACES PER 161(E)(4), 153(a)(6), 154(b)(2), AND 307(H)(2)(I)
- MAXIMUM LOT COVERAGE PER 249.78(D)(6)
- WIND MITIGATION REQUIREMENTS PER 249.78(D)(9)
- MAXIMUM HEIGHT PER 260
- NARROW STREETS AND ALLEY PER 261.1
- APPARENT MASS REDUCTION PER 270(H)
- HORIZONTAL MASS REDUCTION IN LARGE LOTS PER 270.1
- MID-BLOCK PEDESTRIAN PATHWAY 270.2

REQUESTED INCENTIVES:

- LIVING AND SOLAR ROOFS AND LIVING WALLS PER 249.78(D)(4) AND 307(H)(1)(E)
- CURB CUTS ON TRANSIT PREFERENTIAL STREET PER 155(R)(3)

PER PROPOSITION X AND 202.8(A)(2), PROJECTS WITH EXISTING PDR ON SITE THAT WERE PREVIOUSLY ZONED SLI ARE REQUIRED TO PROVIDE A MINIMUM REPLACEMENT OF 75% OF EXISTING PDR IN NEW PROJECT. 202.8(F)(6) ALLOWS NEW PROJECTS TO BE EXEMPT FROM PDR REPLACEMENT REQUIREMENT IF PROJECT RECEIVES AFFORDABLE HOUSING CREDITS FROM SOUTH BEACH MARINA APARTMENTS (BS RESOLUTION 197-16). THE PROJECT SPONSOR IS INVESTIGATING THE USE OF THESE CREDITS AND INCLUDED IN THIS DRAWING PACKAGE ARE ALTERNATE PLANS FOR THE GROUND FLOOR AND A SINGLE BASEMENT TO ILLUSTRATE HOW THE PROJECT WOULD CHANGE IF PDR IS NOT PROVIDED IN THE PROJECT.

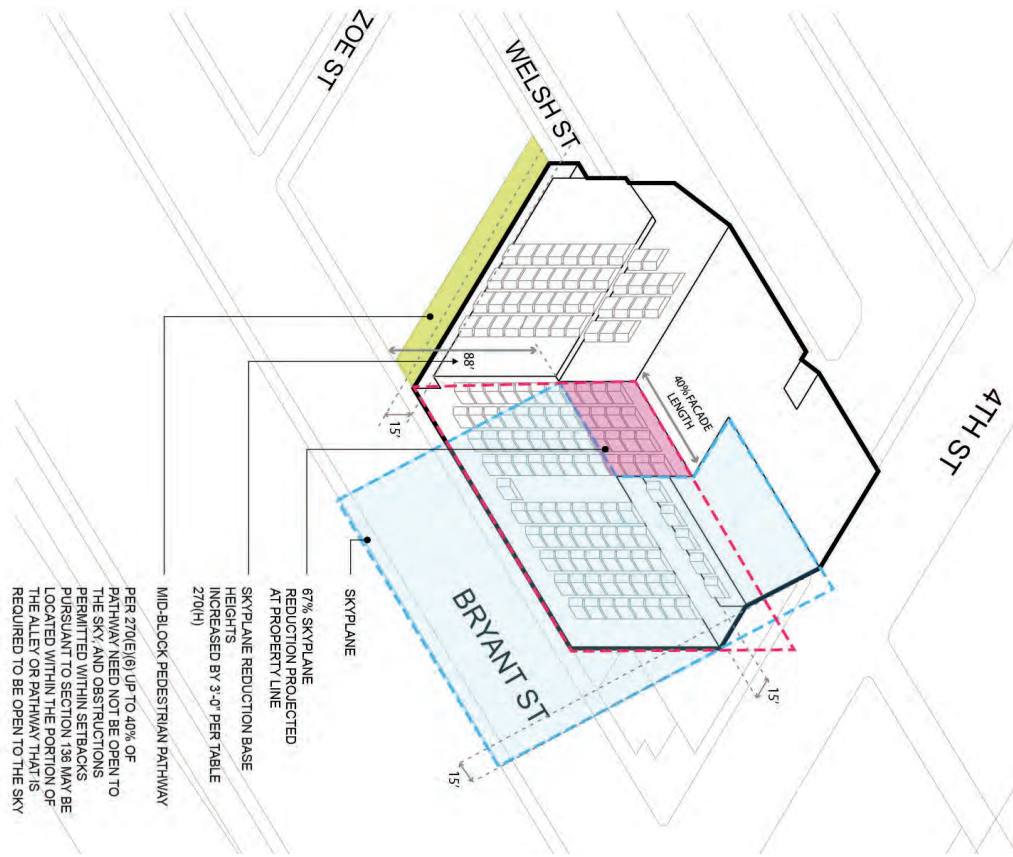
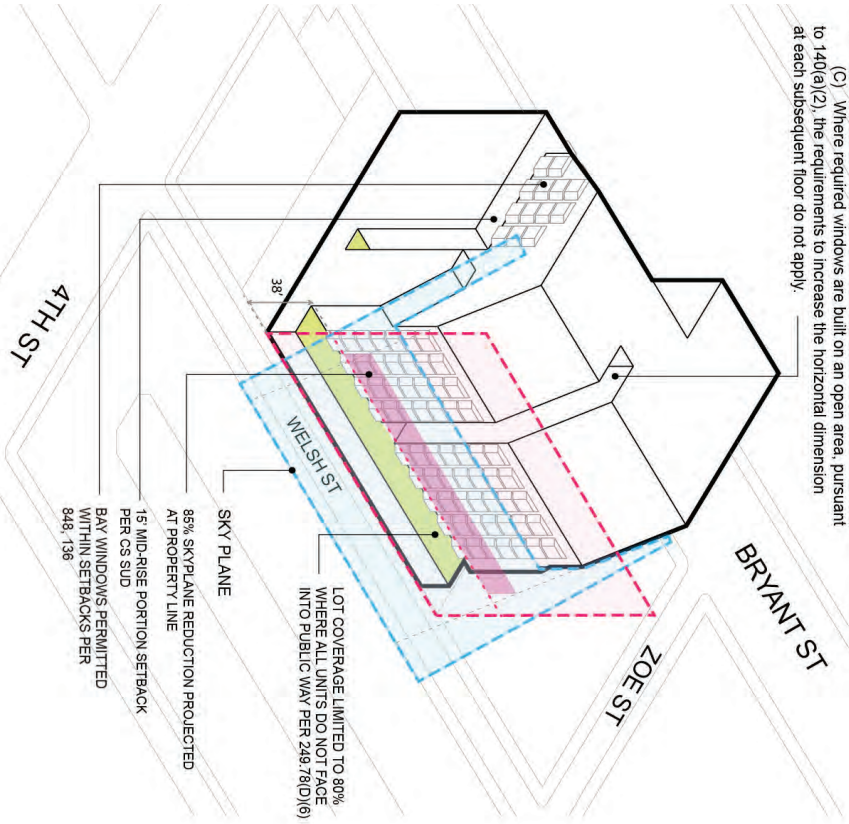
PROJECT DESCRIPTION

Housing Sustainability District Application Drawings
555-585 Bryant Street
San Francisco, CA 94107

05-27-2021

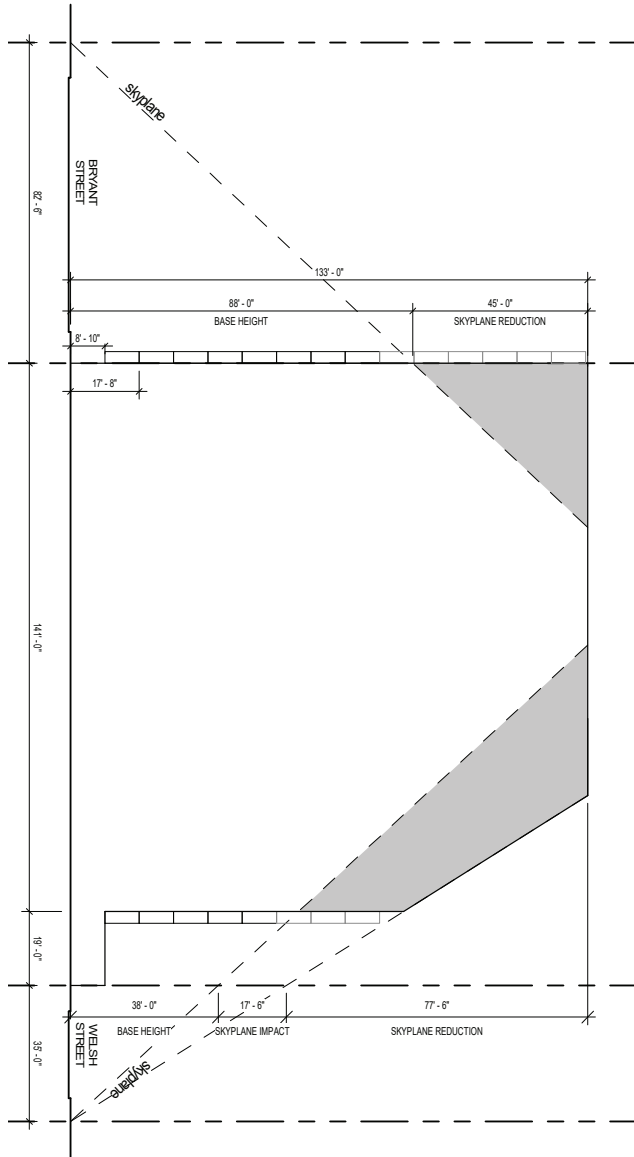
270(d)(11) Dwelling Unit Exposure. The requirements of Section 140 shall apply, except that the required windows (as defined by Section 504 of the San Francisco Housing Code) of at least one room that meets the 120-square-foot minimum superficial floor area requirement of Section 503 of the Housing Code shall face on an open area as follows:

- (A) For units constructed above 85 feet in height, the required windows shall face directly on an open area that is no less than 15 feet by 15 feet;
- (B) 10% of units constructed at or below 85 feet may face directly onto an open area that is no less than 15 feet by 15 feet; and
- (C) Where required windows are built on an open area, pursuant to 140(a)(2), the requirements to increase the horizontal dimension at each subsequent floor do not apply.



ZONING ENVELOPE - BASELINE
 Housing Sustainability District Application Drawings
 555-585 Bryant Street
 San Francisco, CA 94107

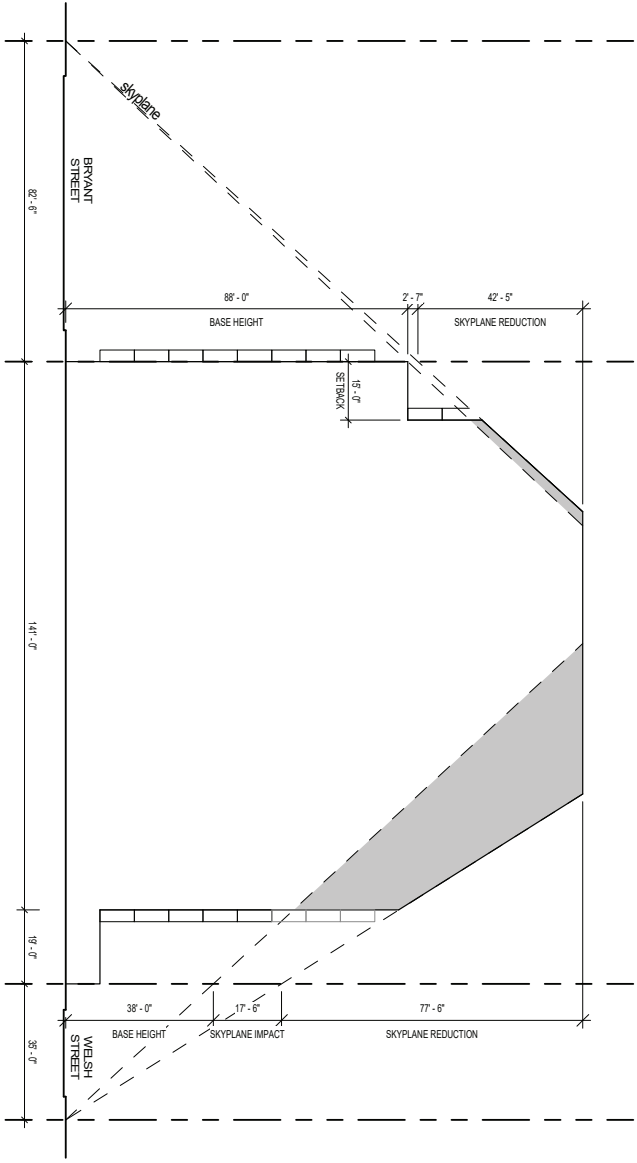
05-27-2021



BUILDING SECTION - BASELINE
 Housing Sustainability District Application Drawings
 555-585 Bryant Street
 San Francisco, CA 94107

05-27-2021

A - 013



BUILDING SECTION - BASELINE
 Housing Sustainability District Application Drawings
 555-585 Bryant Street
 San Francisco, CA 94107

05-27-2021

A - 014

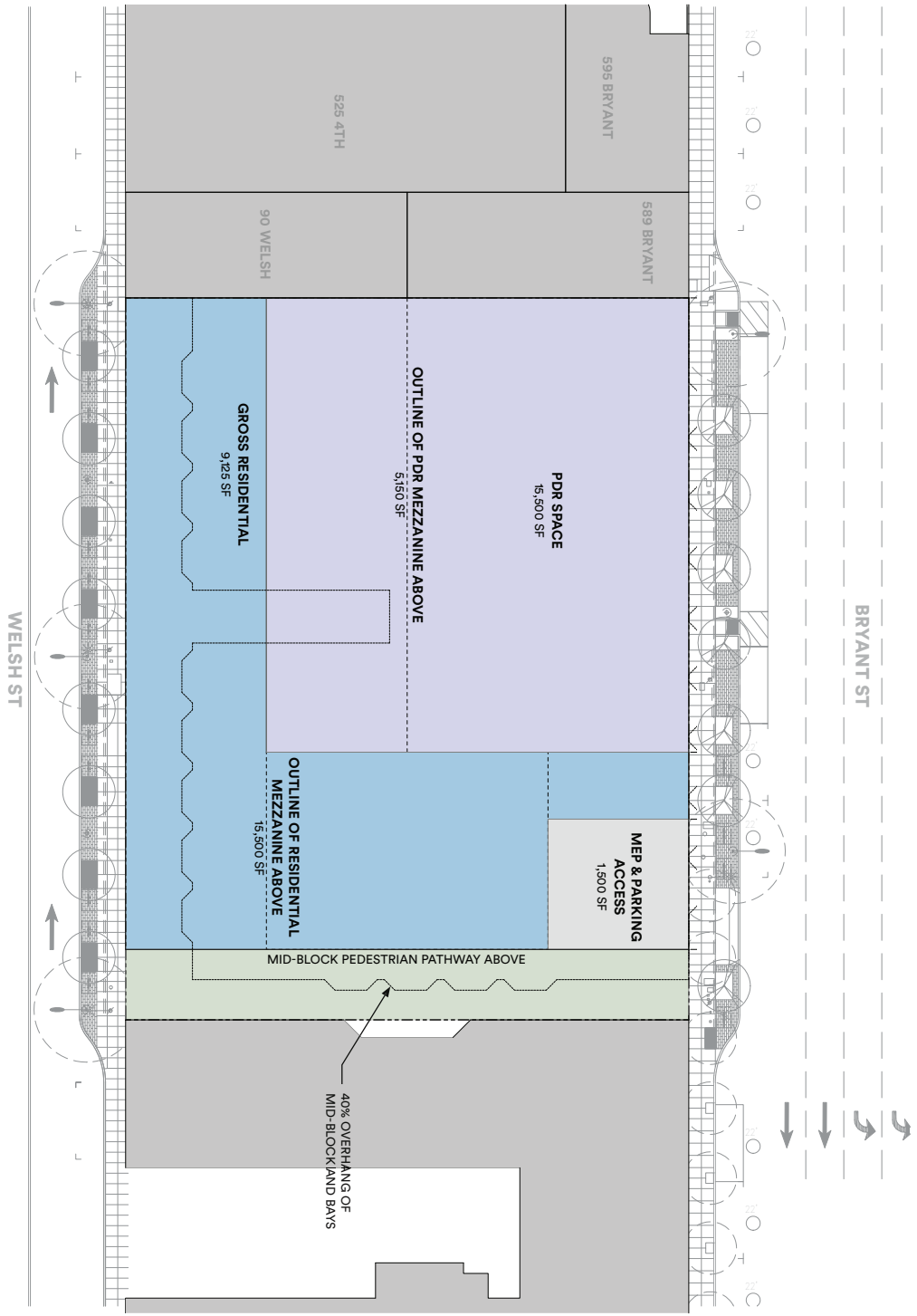
TOTAL SITE AREA:
80% LOT COVERAGE:*

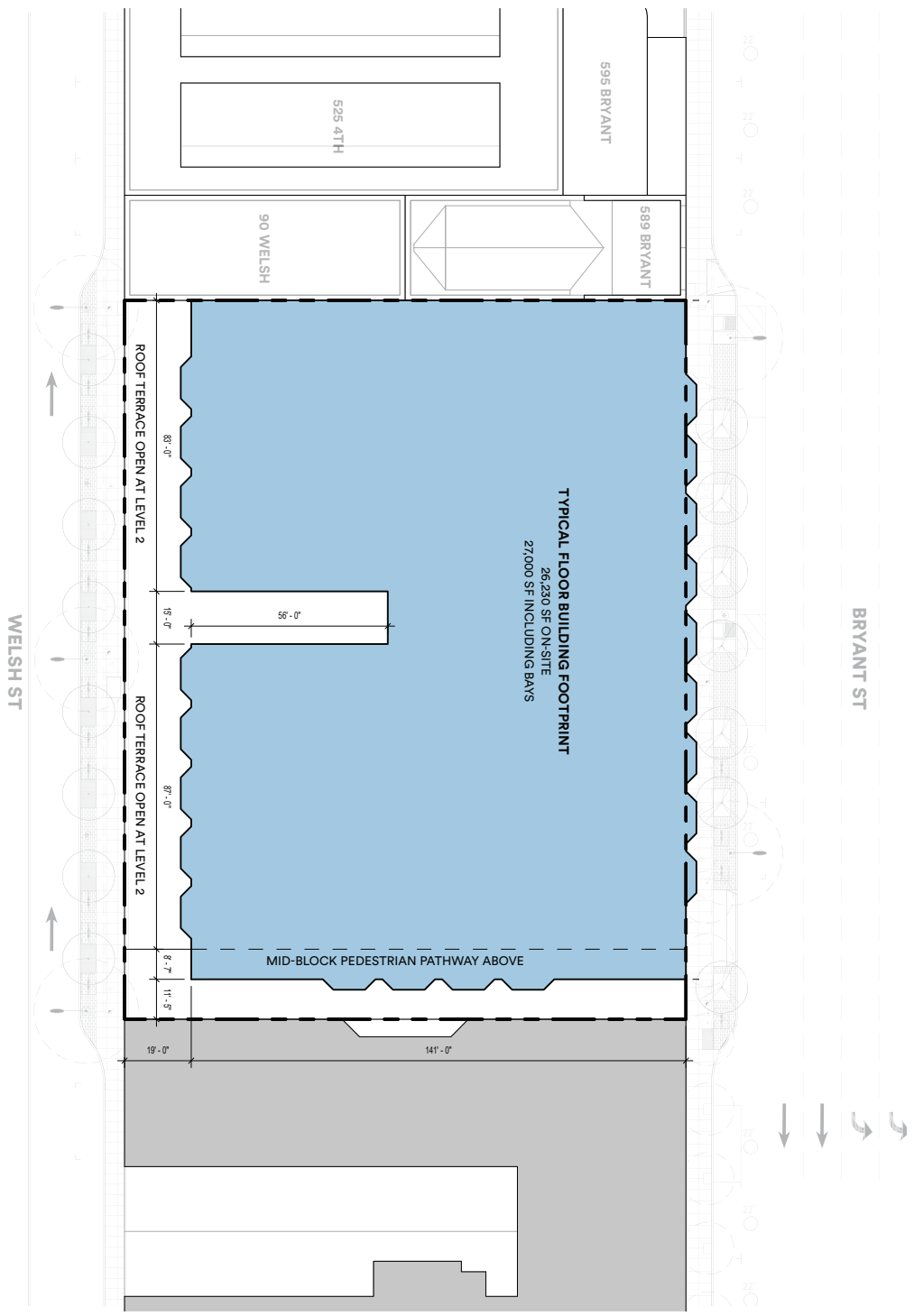
32,800 sqft
26,240 sqft

FLR. ELEV.	F/F	RES GSF	% Lot Coverage	EXIST PDR GSF	NEW PDR NSF	MID-BLOCK PASSAGE GSF	PARKING SPACES	LOADING GSF	TOTALS GSF
+130.00	15.0								
+116.00	14.00	16,345	49%						16,345
+107.00	9.00	18,495	55%						18,495
+98.00	9.00	20,375	61%						20,375
+89.00	9.00	21,665	64%						21,665
+80.00	9.00	22,435	67%						22,435
+71.00	9.00	26,650	79%						26,650
+62.00	9.00	26,805	79%						26,805
+53.00	9.00	26,960	80%						26,960
+44.00	9.00	27,010	80%						27,010
+35.00	9.00	27,010	80%						27,010
+26.00	9.00	27,010	80%						27,010
+17.00	9.00	27,010	80%						27,010
+0.00	17.00	12,706	90%	27,471	5,150	3,200	0	1,440	29,600
+10.00	10.0								
		304,707		27,471	20,604		0	1,440	326,751
		GSF		GSF	NSF	GSF	SPACES	GSF	GSF

*Residential area includes 230 bay windows projecting across propertylines and into setbacks as allowed by code.
Bay windows area doesn't conflict with the 80% lot coverage or setback requirements.

304,707 for bonus
457,061 w/ bonus





Proposed Project

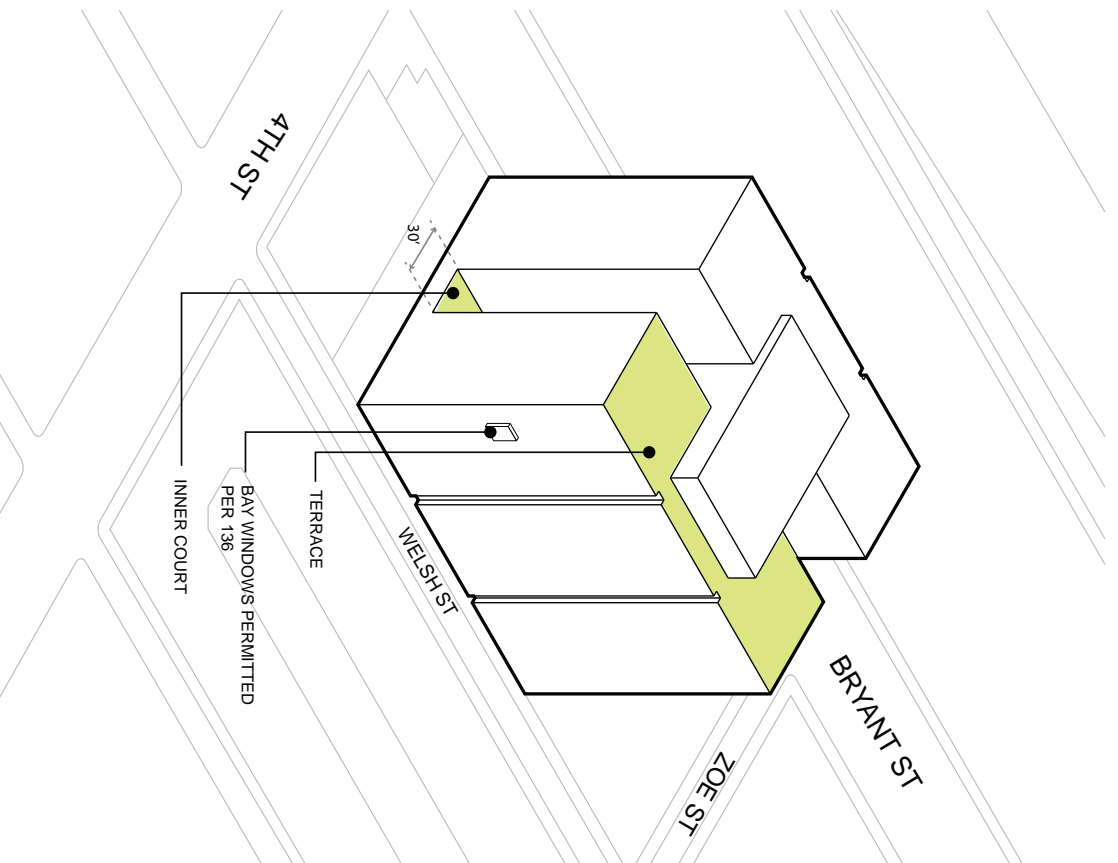
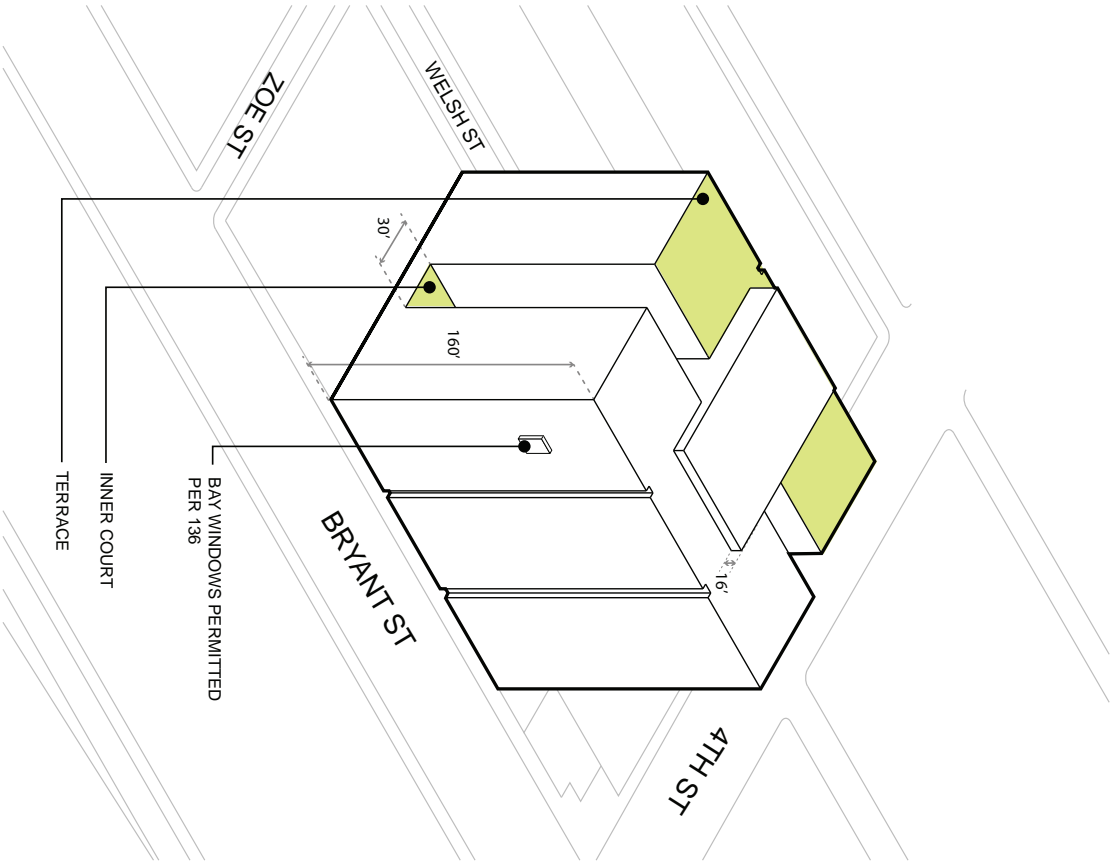
GENERAL ZONING INFORMATION	REFERENCE	NOTES
ZONING USE DISTRICT	ZNI	CMUD - Central SoMa-Mixed Use Office
HEIGHT & BULK DISTRICT	HT1	130-CS
SPECIAL USE DISTRICT	SU1	Central SoMa; Within 1/4 Mile of an Existing Fringe Financial Service
OVERLAY PLAN OR OTHER DRAFT CONTROLS?	SFPI Map	Filipino Cultural Heritage District, Proposition X 2016
EXISTING PDR ON SITE	202.8(a)(2), 202.8(f)(6)	Existing Quantity TRC - 75% of existing PDR area must be replaced in project as PDR per MUD zoning Exempt from PDR requirement if project receives affordable housing credits from South Beach Marina Apartments (BS) resolution 197-
HEIGHT AND BULK CONTROLS		
SITE AREA	270	32, 640 sqft
HEIGHT LIMIT	132.4, 270	133 FT (130-CS plus 3'-0" per 249.78(c)(5) and 270(h))
BULK LIMITS	132.4	CS - Streetwall and Property line setbacks per 132.4, Skyplane mass reductions per 270(h)
REQUIRED SETBACKS	132.4	15 FT above 88' at ROW and interior property lines (85 FT street wall height plus 3'-0" for project with PDR per 249.78(c)(5) and 270(f))
REAR YARDS / COURTS	249.78(d)(6)	80% Max Lot coverage at levels where all residential units do not face into public right of way
MID-BLOCK PEDESTRIAN PATHWAY REQUIREMENTS	270.2(d)(2)	For sites longer than 200 FT where criteria met
UNIT SEPARATION / EXPOSURE REQUIREMENTS	249.78(d)(1)	Inner court 15 FT x 15 FT min for all units above 85 and 10% of units below 85 FT; req's of 140(d)(2) to increase width do not apply
DETAILED CONTROLS & REQUIREMENTS		
RESIDENTIAL DENSITY LIMITS	848	None
UNIT MIX CONTROLS	848/207.6	40% 2bd or more, 30% 3bd, or 35% 2bd or more and 10% 3bd.
OPEN SPACE REQUIREMENTS	848	80 sqft per unit (private); 54 sqft per unit (if publically accessible)
OBSTRUCTIONS OVER STREETS AND ALLEYS	136(c)(2)	Maximum width of bay windows is 15'-0"
GROUND FLOOR MINIMUM FLOOR HEIGHT	249.78(d)(7)	PDR space to have minimum 7'-0" floor to floor measured from grade
CURB CUT RESTRICTIONS	155(f)(3)	Curb cuts no permitted on Transit Preferential Street
OFF-STREET PARKING - RESIDENTIAL	151.1	No parking required; 0.25 per unit Permitted, 0.5 per unit Conditional, car share Required
OFF-STREET LOADING - RESIDENTIAL & OFFICE	152	2 Loading Spaces Required - Substitution of two service vehicle spaces for off-street loading space may be made per 153(a)(6)
BICYCLE REQUIREMENTS - RESIDENTIAL	155.2	1 Loading space required
OFF-STREET LOADING - PDR	155.2	Class 1: 100 spaces +1 space for every 4 dwelling units over 100 (200) Class 2: 1 per 20 dwelling units (50)
CAR SHARE REQUIREMENTS - RESIDENTIAL	166	4 Car Share spaces Required

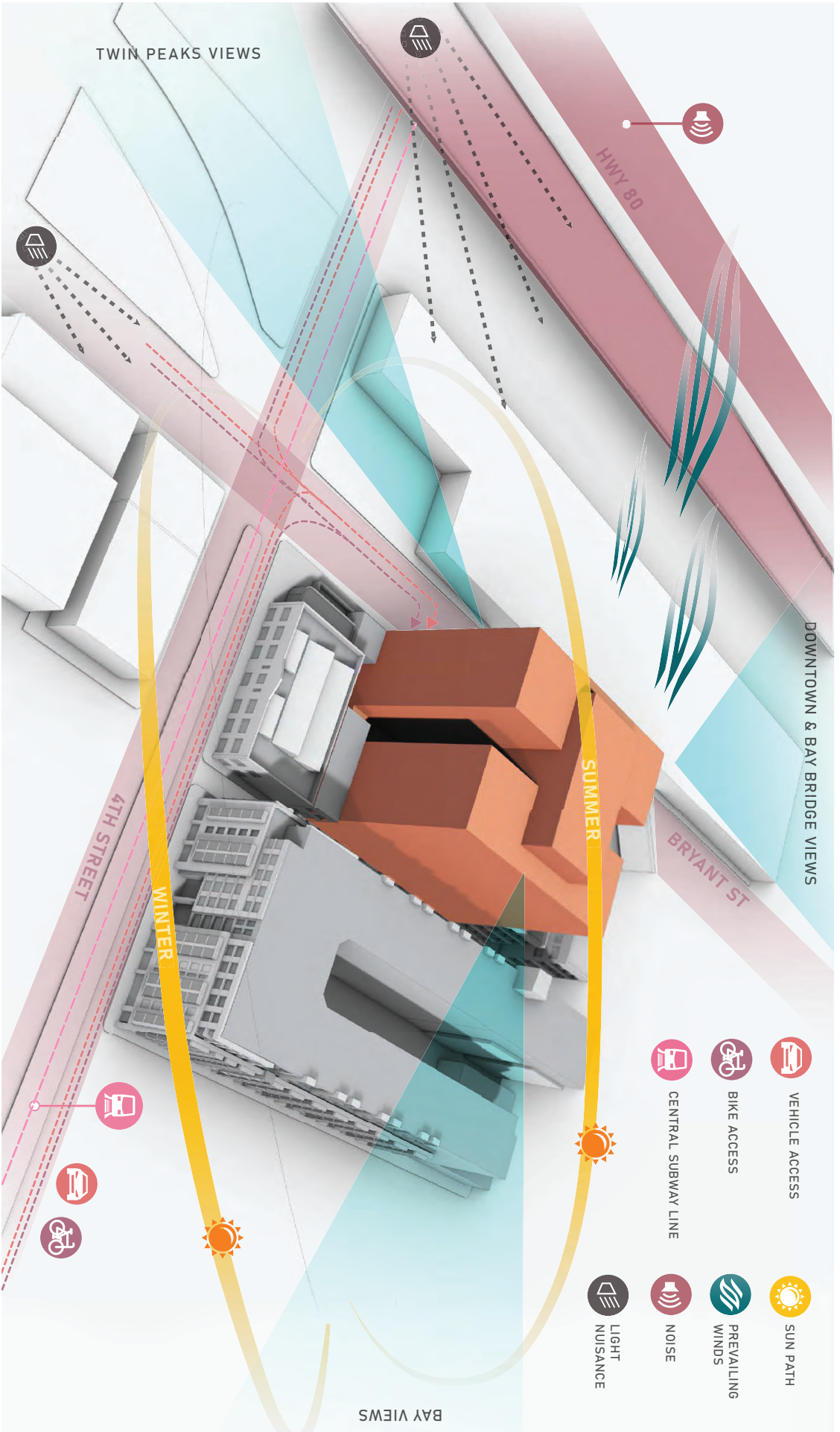
ZONING INFORMATION

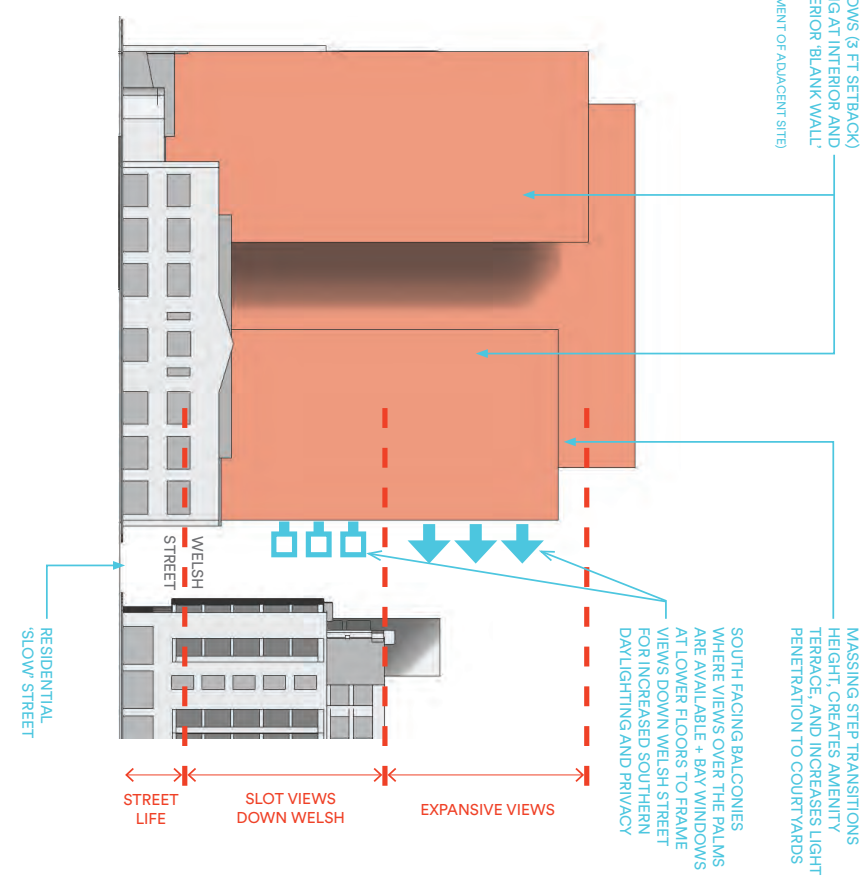
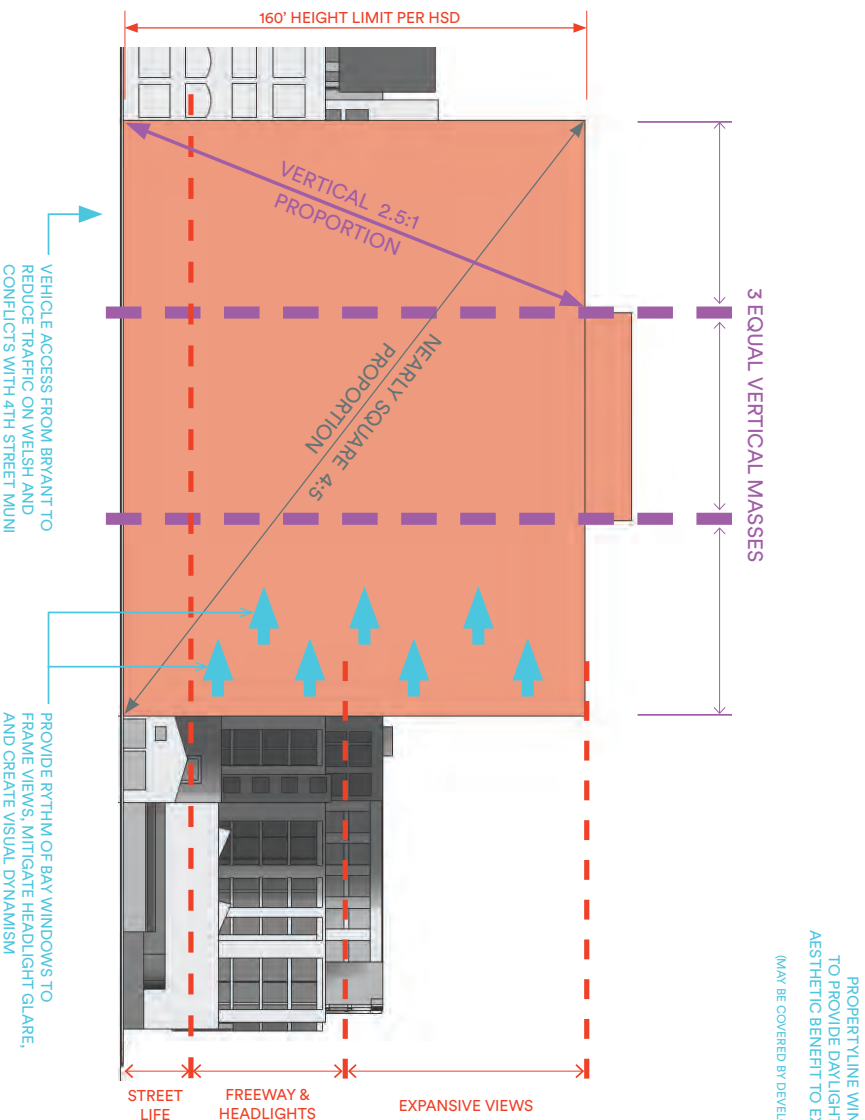
Housing Sustainability District Application Drawings
555-585 Bryant Street
San Francisco, CA 94107

05-27-2021

A - 020





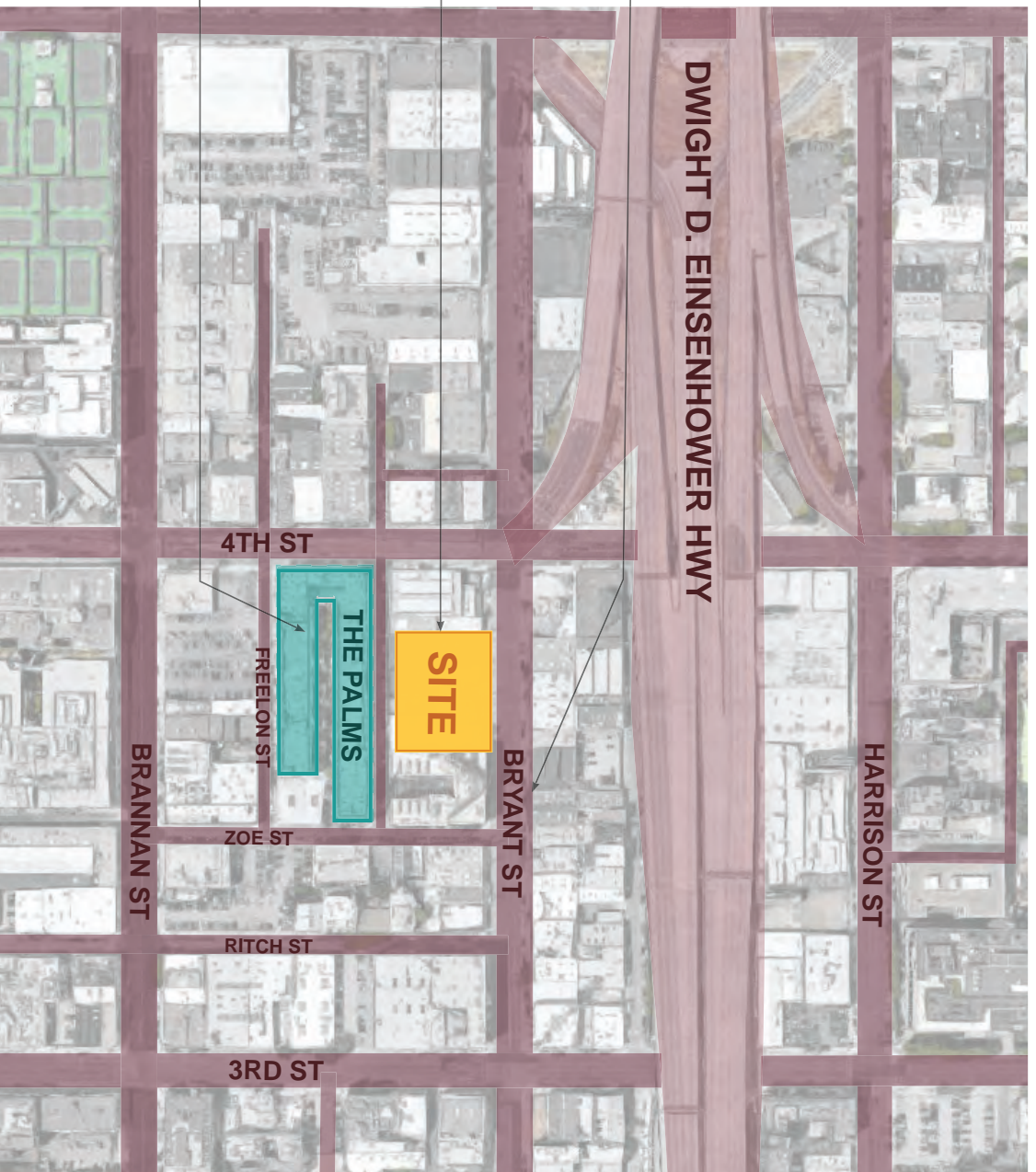


555-585 BRYANT IS PART OF A BLOCK THAT IS BETWEEN A FULL BLOCK LONG RESIDENTIAL PROJECT (THE PALMS) TO THE SOUTHEAST, AND BRYANT STREET TO THE NORTHWEST. THE EISENHOWER FWY (US 80) BEYOND BRYANT CONTRIBUTES TO WHAT IS A VERY HIGH VOLUME OF TRAFFIC ON BRYANT MAKING IT UNSUITABLE FOR PEDESTRIAN CROSSINGS. THE PALMS SPANS THE ENTIRE BLOCK FACE FROM 4TH TO ZOE, EFFECTIVELY BLOCKING ANY CONNECTION WITH ALLEYS OR PEDESTRIAN LANES, AND NONE CURRENTLY EXIST.

GIVEN THE HIGH VOLUME OF VEHICULAR TRAFFIC AT BRYANT THERE IS NO OPPORTUNITY FOR CONNECTION WITH ANY FUTURE PATTERN OF ALLEYS TO THE NORTHWEST OF THE SITE.

PROJECT SITE IS 205' IN LENGTH, HOWEVER THE BLOCK IS IN AN AREA THAT HAS NO SURROUNDING PATTERN OF ALLEYS.

DUE TO 'THE PALMS' SPANNING THE ENTIRE BLOCK BETWEEN 4TH ST AND ZOE ST THERE IS NO OPPORTUNITY TO CREATE A PATTERN OF ALLEYS TO THE SOUTHEAST THE SITE.







GROUND FLOOR AT BRYANT STREET
STREET TREE REMOVED FOR CLARITY



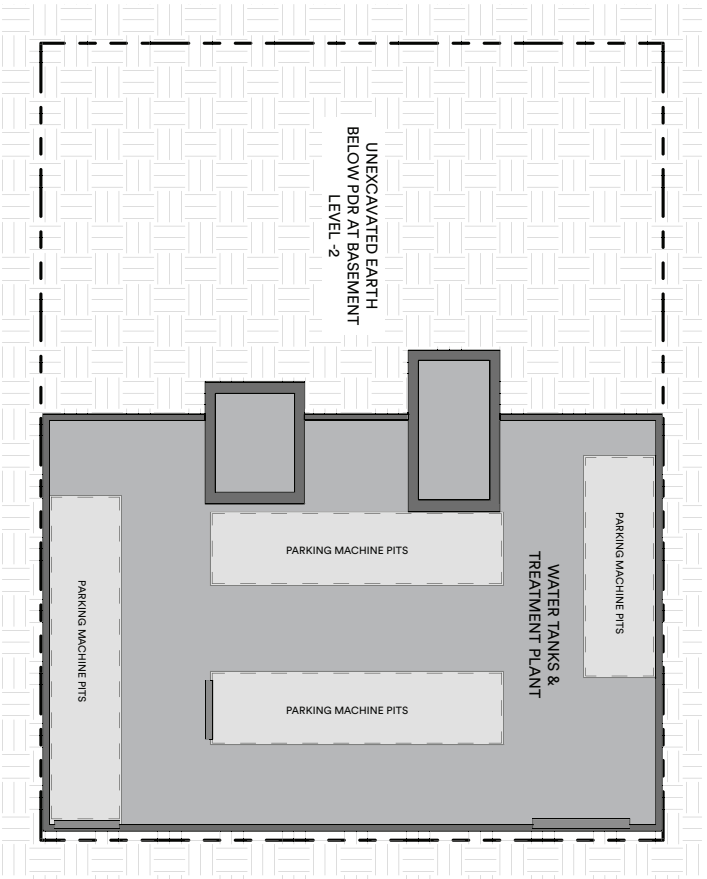
VIEW OF GROUND FLOOR AT WELSH STREET



INDIVIDUAL GLAZED SEGMENTS TO BE
LESS THAN 24 SQFT EACH TO ELIMINATE
BUILDING-FEATURE RELATED BIRD HAZARDS



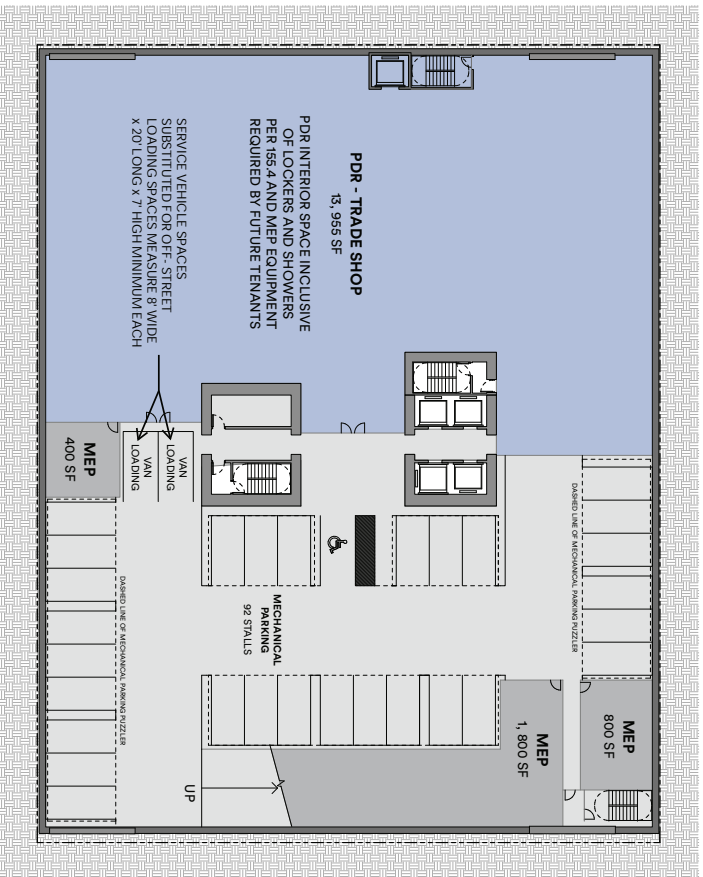
VIEW FROM DWIGHT D. EISENHOWER HWY



BRYANT ST
(ABOVE)

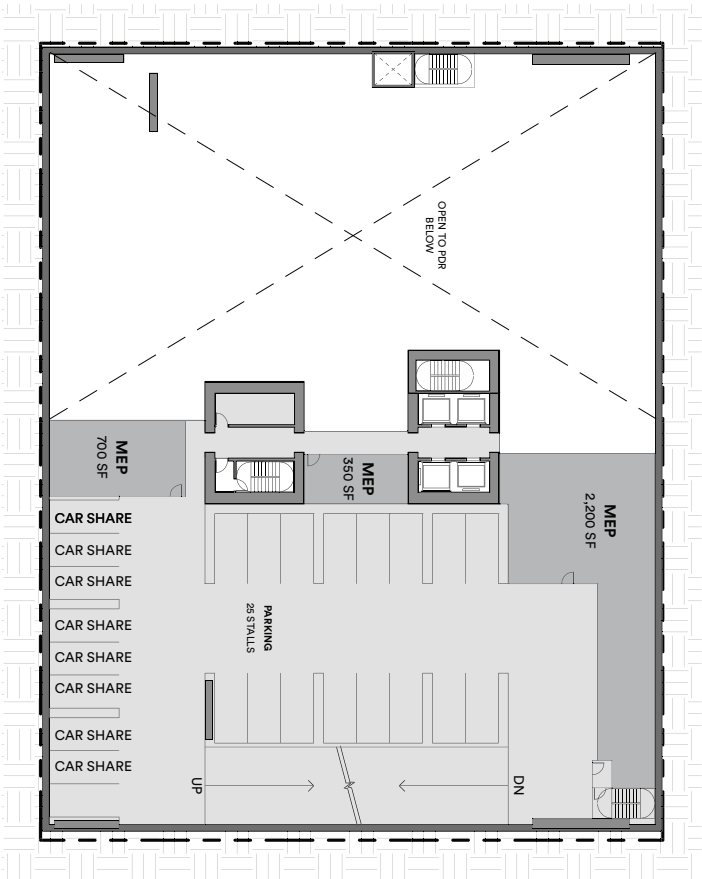
WELSH ST (ABOVE)

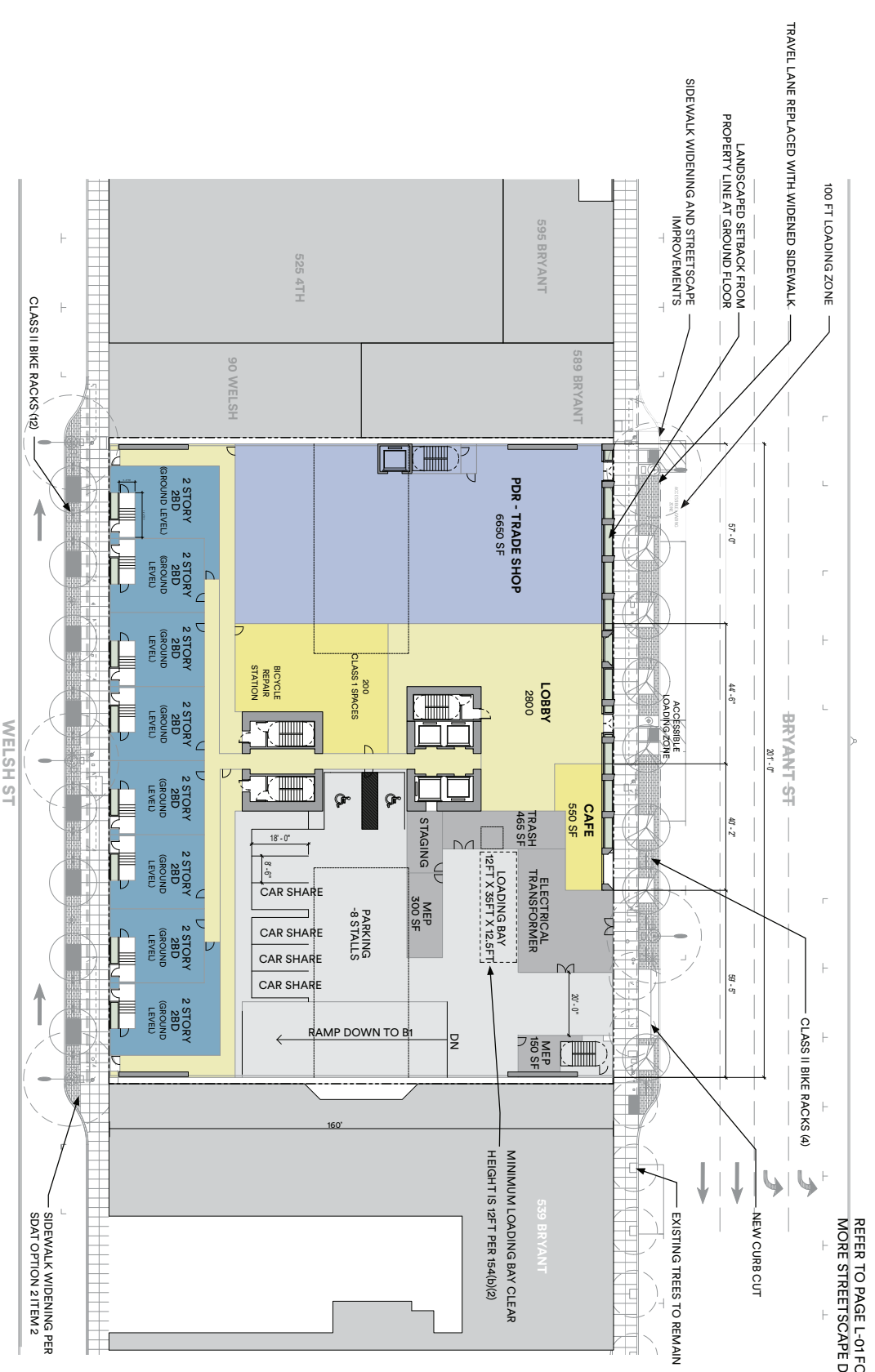
BRYANT ST
(ABOVE)



WELSH ST (ABOVE)

Fire Pump is not required to be diesel powered and will be electrically powered.





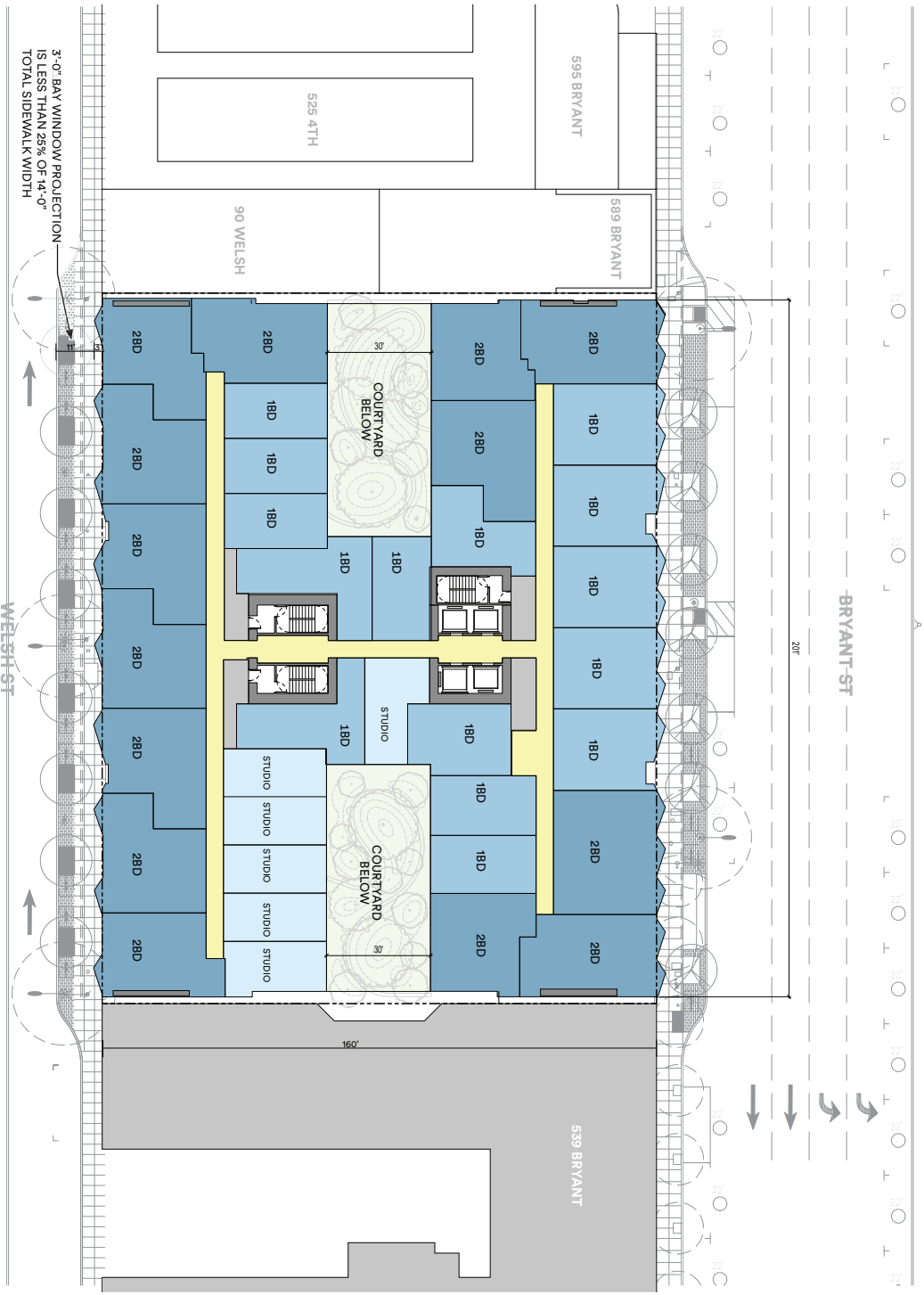
REFER TO PAGE L-01 FOR MORE STREETSCAPE DETAIL

Street trees specified for Bryant Street are Lophostemon confertus. The trees specified for Welsh Street are Tristaniopsis laurina.

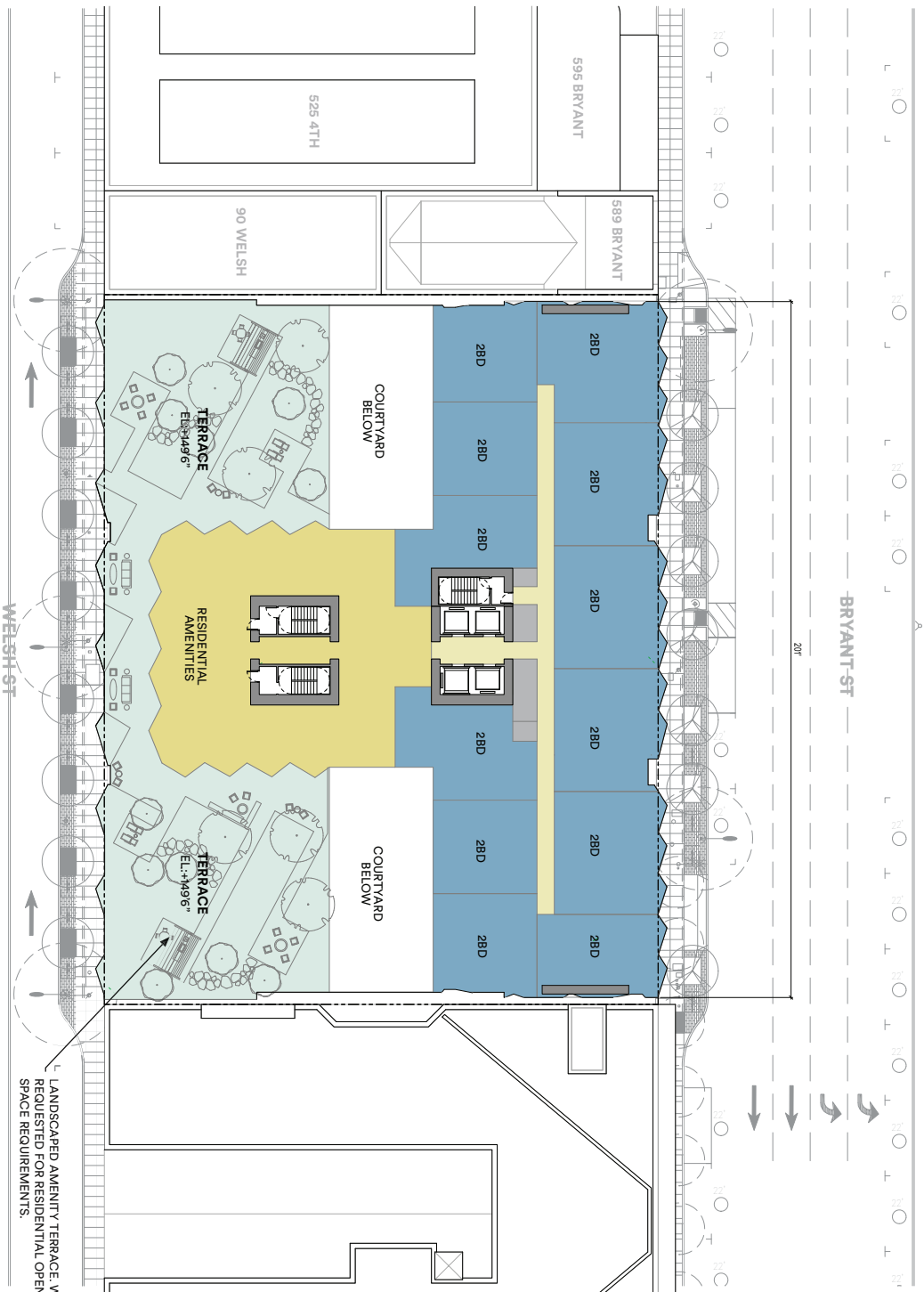
Both species are recommended by San Francisco Public Works' Bureau of Urban Forestry. The height and density of these trees are consistent with the RWD1 wind study, which assumed 15ft - 20ft height and 8ft - 10ft canopy widths.

REFER TO L-04 FOR MORE LANDSCAPE DETAIL

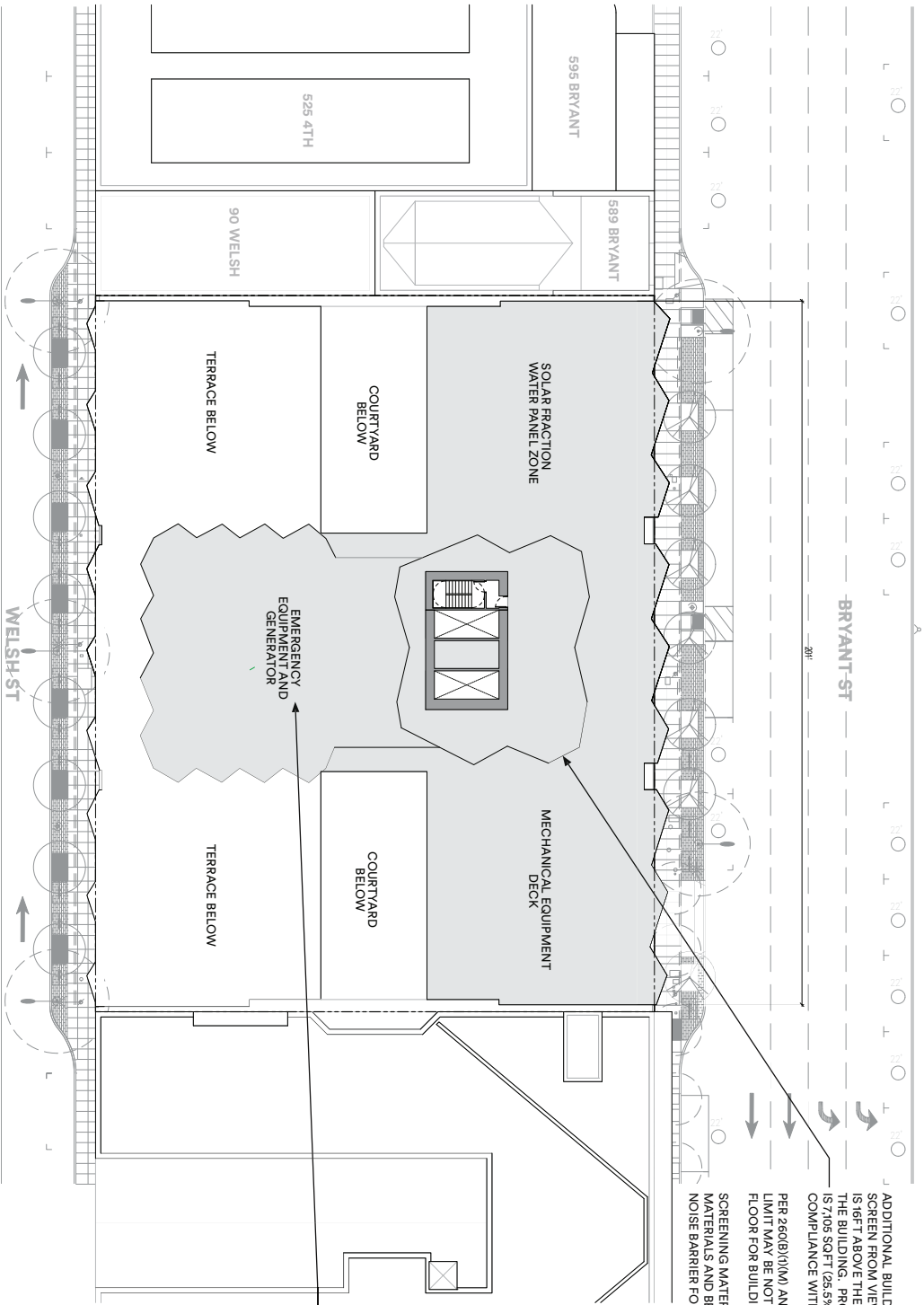




REFER TO L-05 FOR MORE LANDSCAPE DETAIL



REFER TO A-066 FOR LIVING ROOF EXHIBIT



ADDITIONAL BUILDING VOLUME USED TO ENCLOSE AND SCREEN FROM VIEW MECHANICAL AND ELEVATOR EQUIPMENT IS 16FT ABOVE THE 160FT HEIGHT OF THE HIGHEST ROOF OF THE BUILDING. PROPOSED FOOTPRINT OF 16FT SCREENING IS 7,105 SQFT (25.8%) OF OVERALL ROOF AREA (ABOVE L2) IN COMPLIANCE WITH 20%-30% RANGE ALLOWED BY 2801(B). PER 2601(B)(1)(M) AND 141(C) SCREENING AREA ABOVE HEIGHT LIMIT MAY BE NOT MORE THAN 100% OF HIGHEST OCCUPIED FLOOR FOR BUILDINGS NOT TALLER THAN 200 FEET. SCREENING MATERIAL WILL COMPLIMENT OVERALL FACADE MATERIALS AND BE NOT POROUS SO THAT IT SERVES AS A NOISE BARRIER FOR EQUIPMENT. SEE ELEVATIONS.

EMERGENCY GENERATOR TO BE RADIATOR COOLED DIESEL FUL CRUISE ENGINE SET RATED 800K/1000KVA, 277/480V, 3-PHASE, 4 WIRE, 1800RPM LOCATED IN A WEATHERPROOF ENCLOSURE. PER BAAQMD REQUIREMENTS THE GENERATOR WILL BE RATED TIER 4

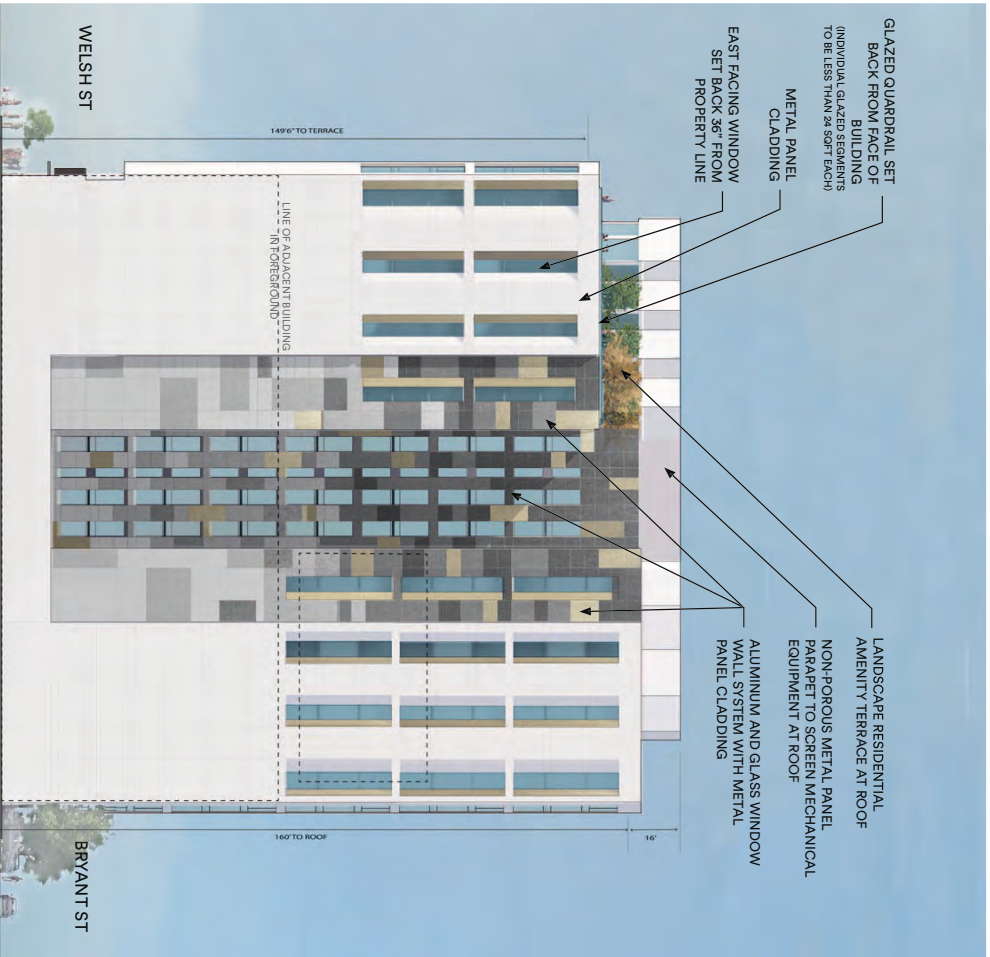




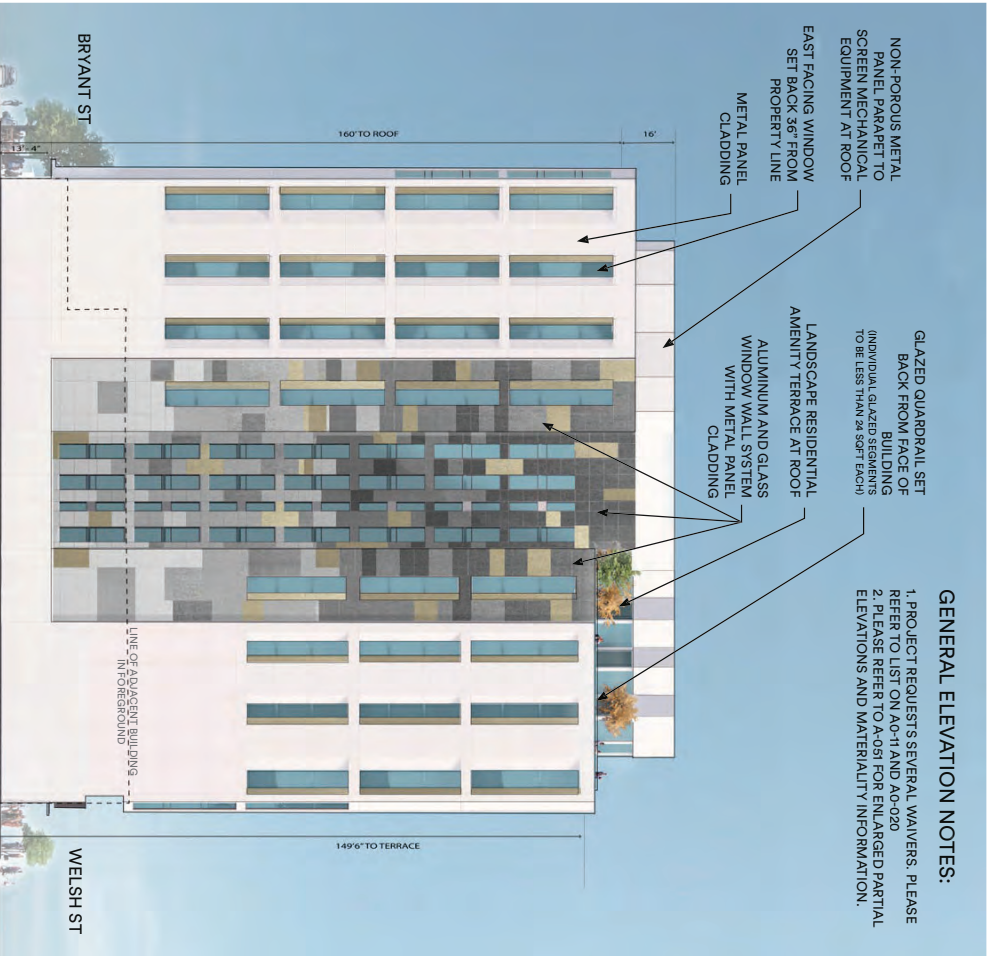
GENERAL ELEVATION NOTES:

1. PROJECT REQUESTS SEVERAL WAIVERS. PLEASE REFER TO LIST ON A-0-11 AND A-0-20
2. PLEASE REFER TO A-0-51 FOR ENLARGED PARTIAL ELEVATIONS AND MATERIALITY INFORMATION.

MASONRY AND WOOD LOOK PANELS AT TWO STORY TOWNHOUSES
 RECESSED PRIVATE STOOPS AT GROUND FLOOR DWELLING UNITS
 STREET TREES AND SIDEWALK FURNISHINGS



EAST ELEVATION



WEST ELEVATION

GENERAL ELEVATION NOTES:
 1. PROJECT REQUESTS SEVERAL WAIVERS. PLEASE REFER TO LIST ON A0-11 AND A0-020
 2. PLEASE REFER TO A-051 FOR ENLARGED PARTIAL ELEVATIONS AND MATERIALITY INFORMATION.



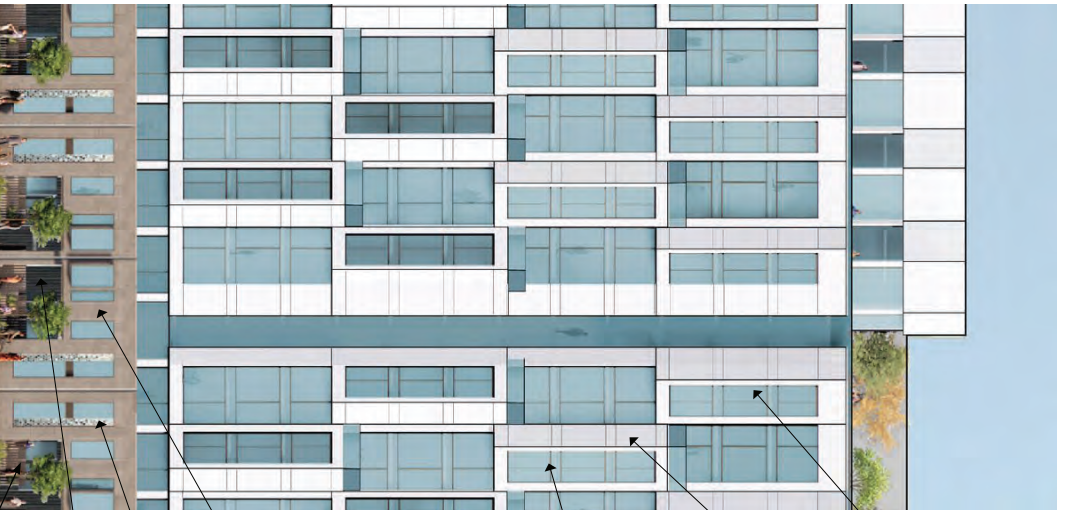
G-2
HIGH PERFORMANCE
GLAZING BLUE-GREY

M-1
WHITE METAL
CLADDING

TH-3
DARK GREY METAL
MULLIONS

G-1
HIGH PERFORMANCE
GLAZING - CLEAR
AT GROUND FLOOR

T-1
MASONRY STYLE CLADDING
ACCENT AT COLLUMNS AND
MEP WALL



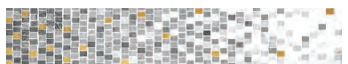
G-2
HIGH PERFORMANCE
GLAZING BLUE-GREY

M-1
WHITE METAL
CLADDING

TH-3
DARK GREY METAL
MULLIONS

TH-2
MASONRY STYLE
CLADDING

T-1
MASONRY STYLE
CLADDING ACCENT
TH-1
WOOD LIKE
CLADDING PANELS
T-1
GATES AND RAILINGS TO
COMPLY WITH 145.1(C)(7)



ACCENT PATTERN



ACCENT COLORS

MATERIAL PALETTE - PROPOSED

Housing Sustainability District Application Drawings
555-585 Bryant Street
San Francisco, CA 94107

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555 BRYANT

MATERIALS BOARD

METAL

- M-1 WHITE METAL CLADDING - WINDOW WALL SYSTEM
- M-2 LIGHT GOLD METAL CLADDING - WINDOW WALL SYSTEM
- M-3 GOLD METAL CLADDING - WINDOW WALL SYSTEM
- M-4 LIGHT GREY METAL CLADDING - WINDOW WALL SYSTEM
- M-5 DARK GREY METAL CLADDING - WINDOW WALL SYSTEM

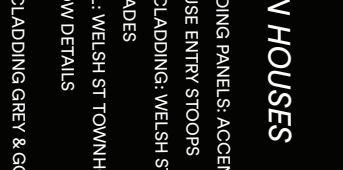
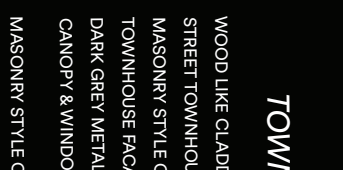
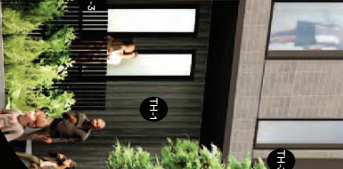
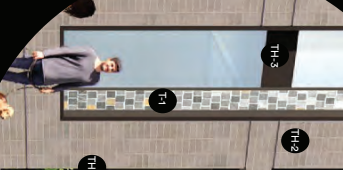
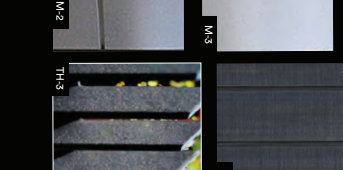
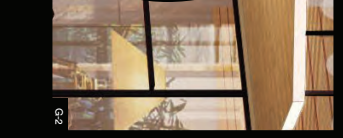
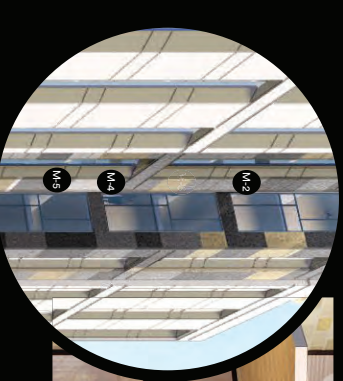
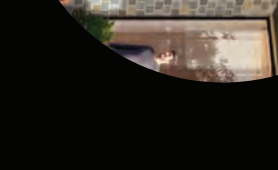
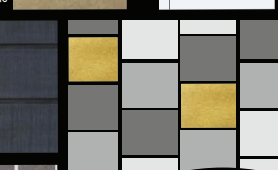
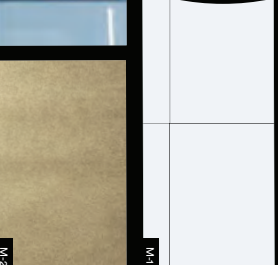
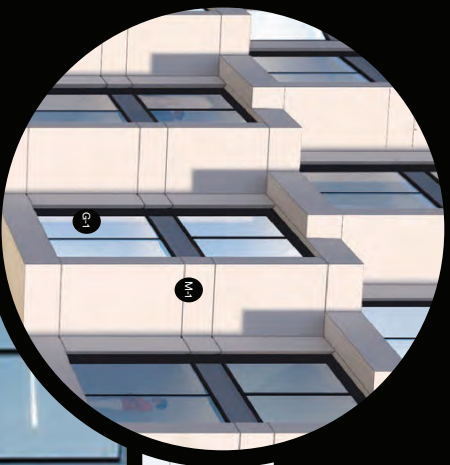
** PAINT COLORS TO MATCH METAL COLORS

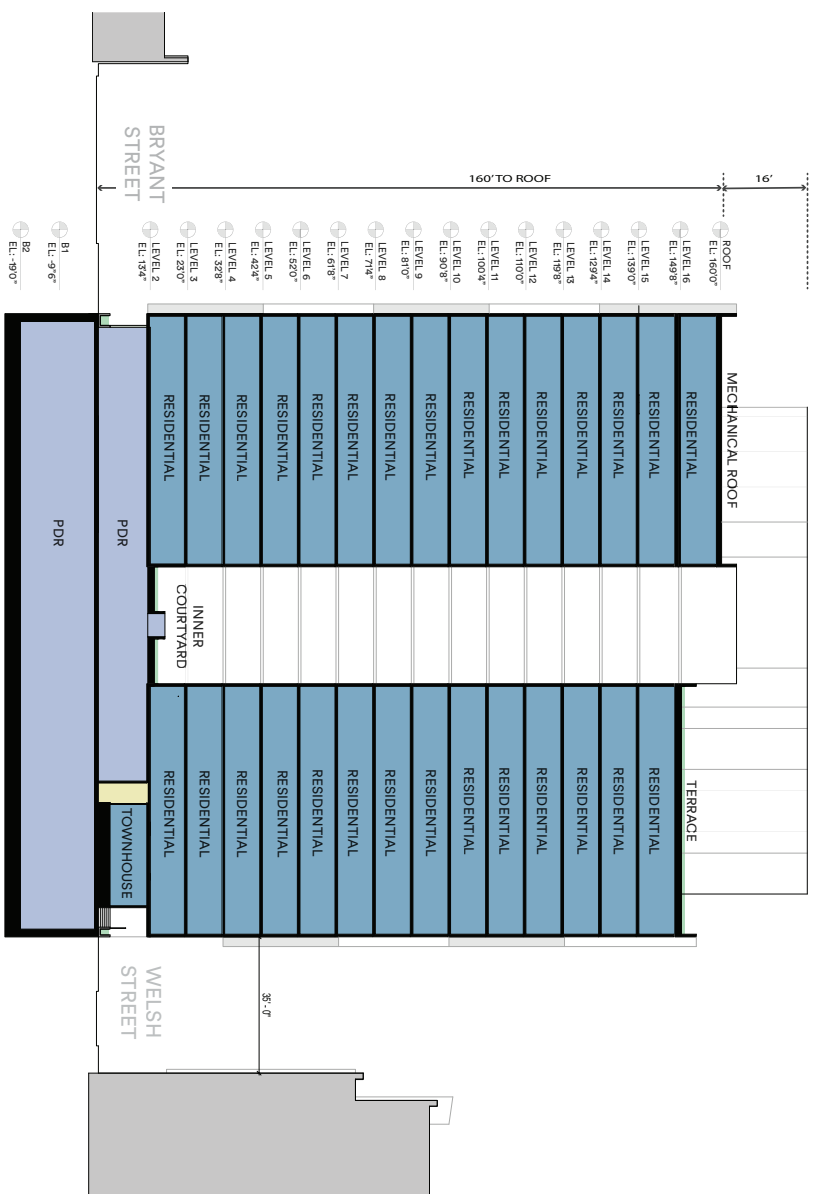
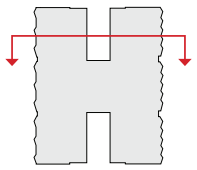
GLASS

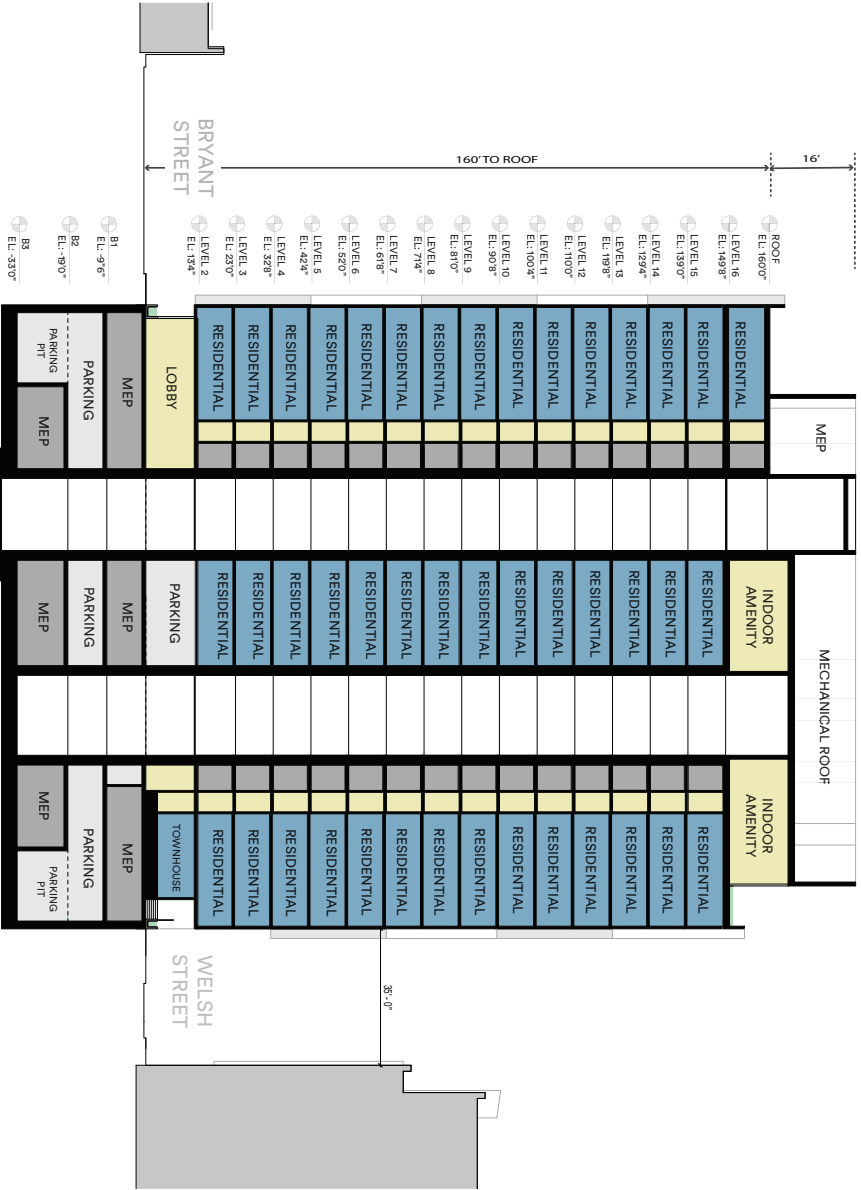
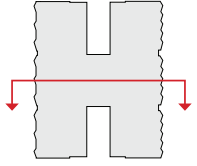
- G-1 HIGH PERFORMANCE GLAZING: BLUE-GREY WITH MEDIUM REFLECTIVITY FOR VISUAL PRIVACY
- G-2 HIGH PERFORMANCE GLAZING: CLEAR WITH LOW REFLECTIVITY FOR MAXIMUM VISUAL TRANSPARENCY

TOWN HOUSES

- TH-1 WOOD LIKE CLADDING PANELS: ACCENT AT WELSH STREET TOWNHOUSE ENTRY STOOPS
- TH-2 MASONRY STYLE CLADDING: WELSH ST TOWNHOUSE FACADES
- TH-3 DARK GREY METAL: WELSH ST TOWNHOUSE FENCING, CANOPY & WINDOW DETAILS
- T-1 MASONRY STYLE CLADDING GREY & GOLD: ACCENT AT BRYANT ST ENTRANCE & WELSH ST TOWNHOUSES





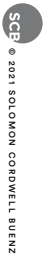


555-585 Bryant STATE DENSITY BONUS AREA TABULATION

TOTAL SITE AREA*: 32,800 SF

FLR. ELEV. F/F	RESIDENTIAL UNITS/FLR.						RESIDENTIAL GROSS GSF			% Lot Coverage	PDR GSF	PARKING + LOADING SPACES	TOTALS GSF
	400-460	500-675	660-715	835-975+	1340-1685	2B TH	RESIDENTIAL GROSS GSF	RESIDENTIAL % Lot Coverage					
+160.0	Roof						3,050					4,000	
+149.7	16	12	0	0	0	12	18,850	57%				18,850	
+139.0	15	35	6	10	5	14	28,300	86%				28,300	
+129.3	14	35	6	10	5	14	28,300	86%				28,300	
+119.7	13	35	6	10	5	14	28,300	86%				28,300	
+110.0	12	35	6	10	5	14	28,300	86%				28,300	
+100.3	11	35	6	10	5	14	28,300	86%				28,300	
+90.7	10	35	6	10	5	14	28,300	86%				28,300	
+81.0	9	35	6	10	5	14	28,300	86%				28,300	
+71.3	8	35	6	10	5	14	28,300	86%				28,300	
+61.7	7	35	6	10	5	14	28,300	86%				28,300	
+52.0	6	35	6	10	5	14	28,300	86%				28,300	
+42.3	5	35	6	10	5	14	28,300	86%				28,300	
+32.7	4	35	6	10	5	14	28,300	86%				28,300	
+23.0	3	35	6	10	5	14	28,300	86%				28,300	
+13.3	2	25	7	10	1	7	28,460	87%				28,460	
+0.0	1	8	0	0	0	8	13,145	N/A	6,650	8	10,050	30,875	
-9.5	B1						2,500			25	14,500	17,000	
-19.0	B2						2,500		13,955	92	15,545	32,000	
-31.0	B3						0		0	0	5,420	11,830	
	TOTAL UNITS	500	85	140	66	201	433,355		20,605	125	45,200	499,085	
			17%	28%	13%	40%				0.25 per unit			

* SITE AREA MEASURED FROM ASSESSOR MAP DIMENSIONS
41.8%

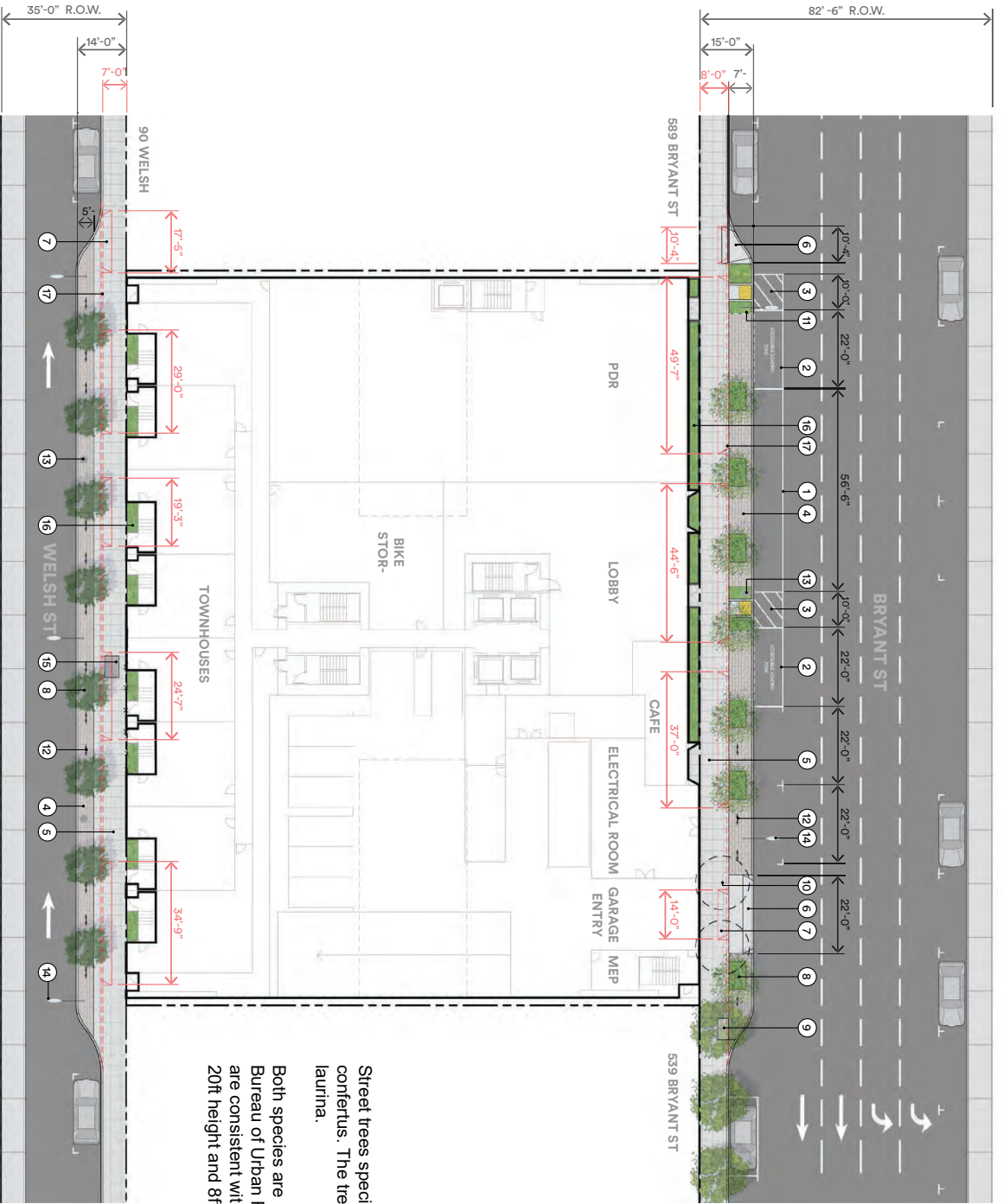


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PROJECT DATA - PROPOSED
Housing Sustainability District Application Drawings
555-585 Bryant Street
San Francisco, CA 94107

05-27-2021

A - 055



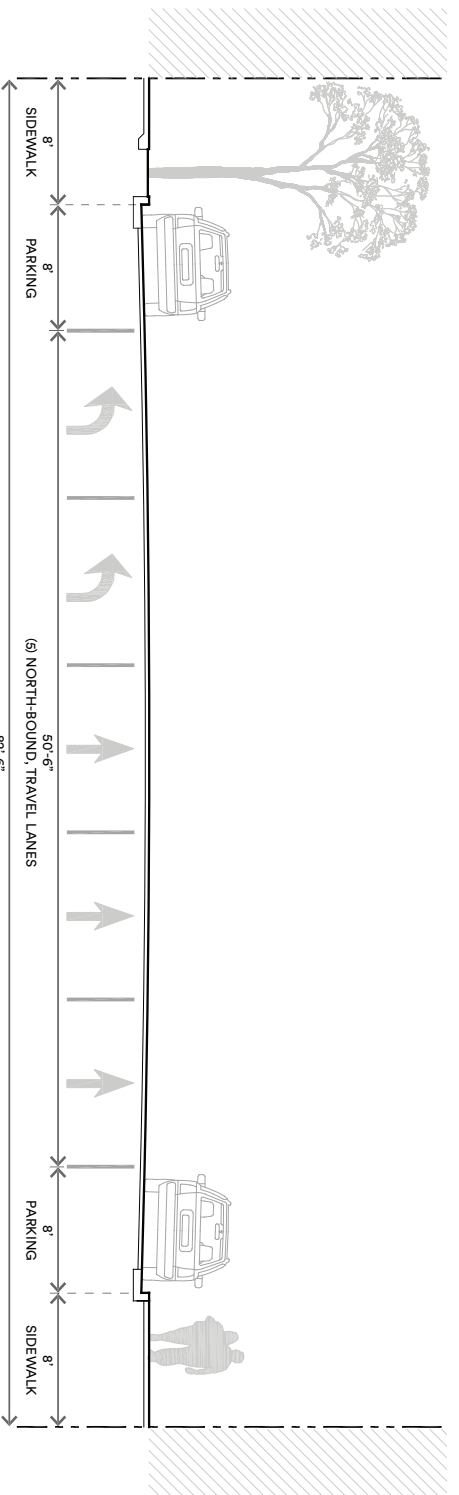
- ① PASSENGER LOADING ZONE
- ② ACCESSIBLE LOADING ZONE
- ③ ACCESSIBLE CLEAR AISLE SPACE AND RAMP
- ④ PERMEABLE PAVERS, FURNISHING AND EDGE
- ⑤ SIDEWALK, POURED-IN-PLACE CONCRETE
- ⑥ (N) DRIVEWAY
- ⑦ (E) DRIVEWAY, TO BE REMOVED
- ⑧ (N) STREET TREES
- ⑨ (E) STREET TREES, TO BE PROTECTED-IN-
- ⑩ (E) STREET TREES, TO BE REMOVED (QTY: 2)
- ⑪ PLANTING AREA IN RIGHT-OF-WAY
- ⑫ (N) BIKE RACK, CLASS 2 (QTY: 16 RACKS/ 32
- ⑬ (N) PEDESTRIAN LIGHTS
- ⑭ (N) ROADWAY LIGHTS
- ⑮ (N) TRANSFORMER VAULT
- ⑯ PLANTING AREA AT BUILDING
- ⑰ (E) CURB, TO BE REMOVED

Street trees specified for Bryant Street are Lophostemon confertus. The trees specified for Welsh Street are Tristanopsis laurina.

Both species are recommended by San Francisco Public Works' Bureau of Urban Forestry. The height and density of these trees are consistent with the RWDI wind study, which assumed 15ft - 20ft height and 8ft - 10ft canopy widths.

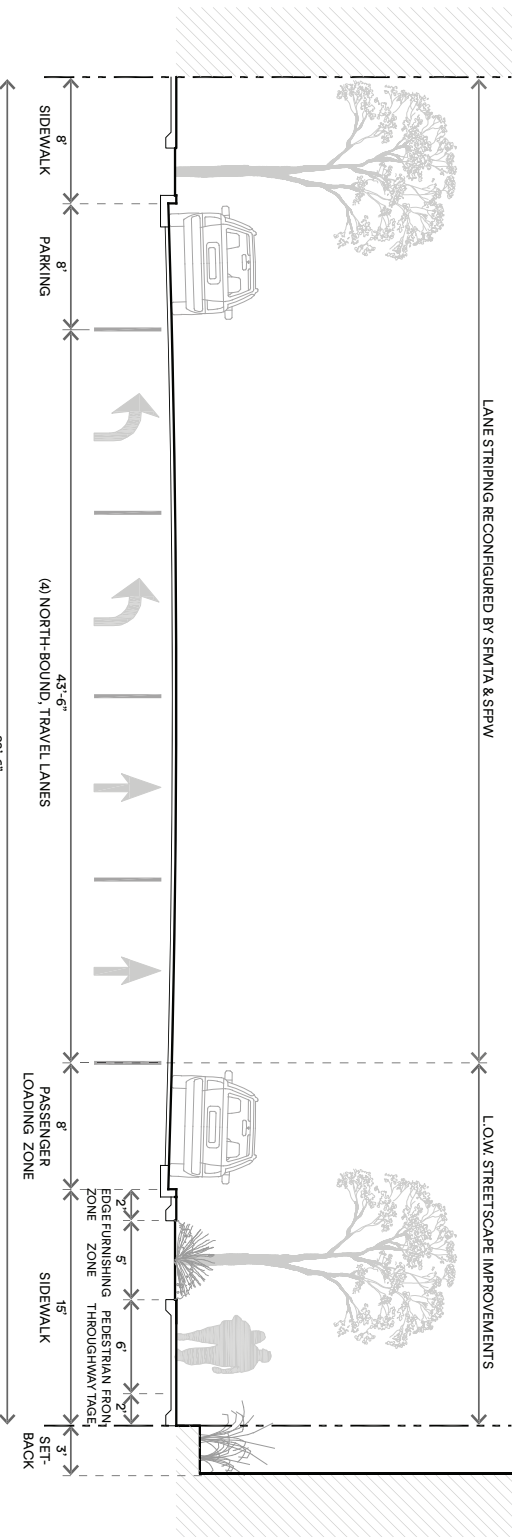
GROUND FLOOR STREETSCAPE PLAN
 Housing Sustainability District Application Drawings
 555-585 Bryant Street
 San Francisco, CA 94107

0 15' 30'
 05-27-2021 L-01



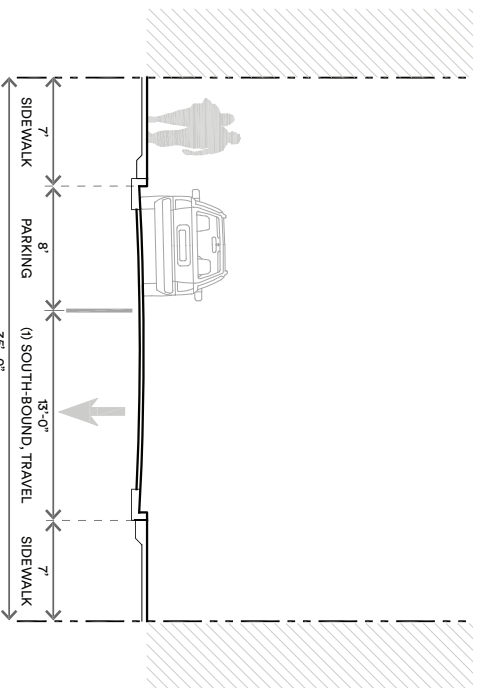
BRYANT STREET R.O.W. - EXISTING

NOTE:
(E) DIMENSIONS ARE
APPROXIMATE



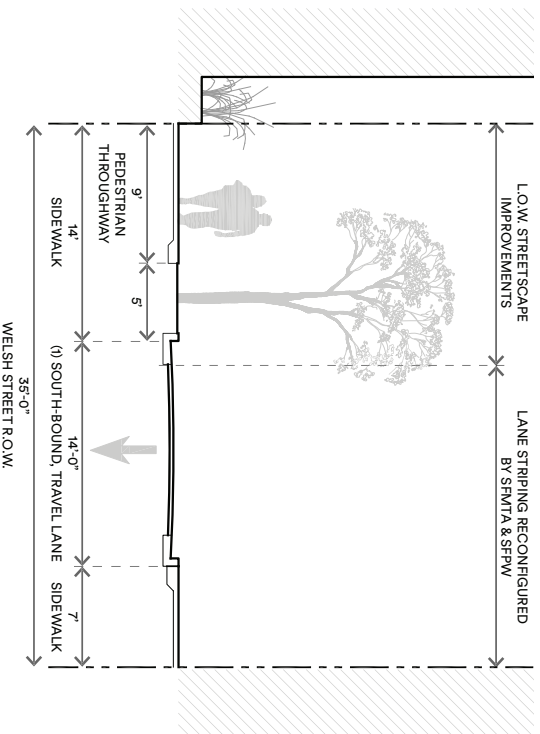
BRYANT STREET R.O.W. - PROPOSED





WELSH STREET R.O.W. - EXISTING

NOTE:
(E) DIMENSIONS ARE
APPROXIMATE



WELSH STREET R.O.W. - PROPOSED



○ REDWOOD GROVE - TALL & VERTICAL TO FILL THE SPACE



② ROLLING LANDFORMS - FILTER VIEWS & CREATE PRIVACY



○ GRAPHIC - CREATE VISUAL INTEREST FROM





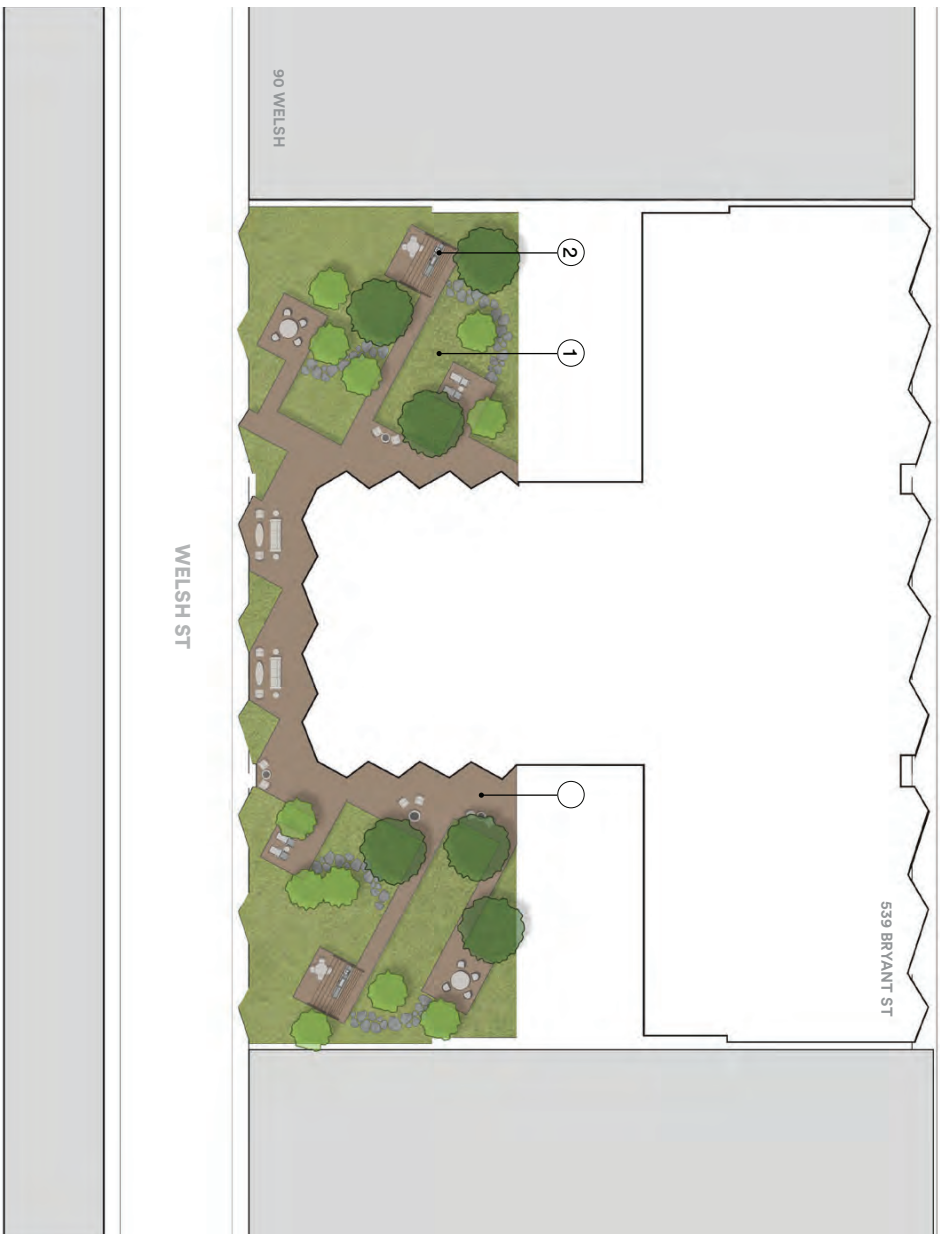
○ NATIVE GRASSLAND - CREATE HABITAT & BLOW IN THE



② PROGRAM DECKS - SMALL GATHERING SPACES FOR DINING &



○ BOARDWALKS - MOVE THROUGH A GARDEN



Appendix

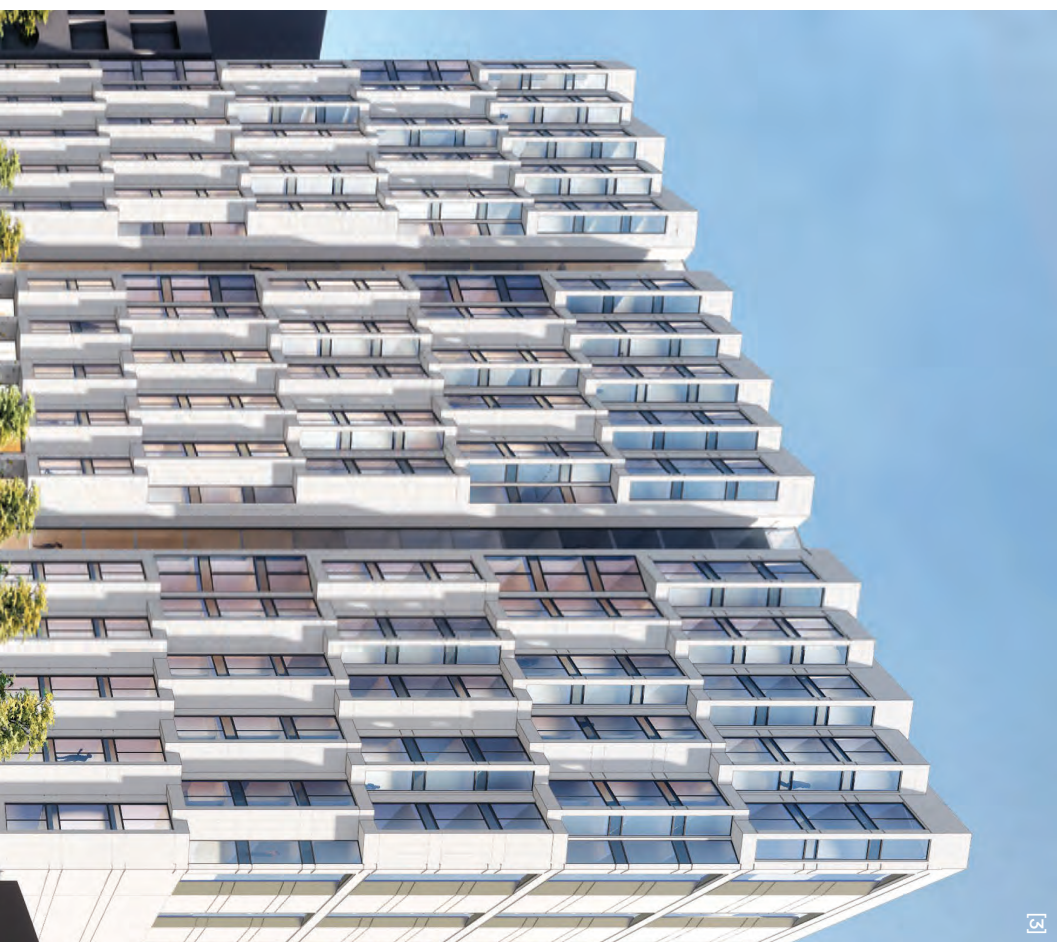
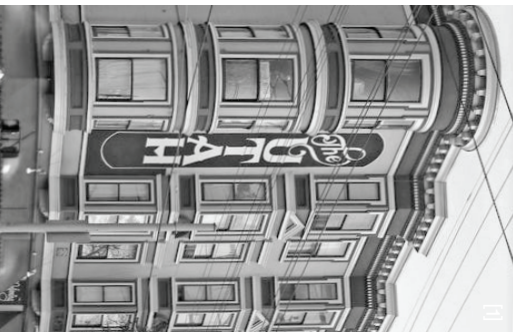
AN ARCHITECTURE THAT RESPONDS

555-585 Bryant combines several parcels into a single clear residential expression. The street facing facades are broken up into a collection of three vertical masses separated by recessed slots [2].

Angled bay windows are composed in an undulating rhythm of 3 story groupings that provide texture and a sense of visual movement while graduating from greater to lesser solidity as the building rises [3]. These bay window groupings compliment the proportions of the facade massing, frame views, and reference the character of surrounding buildings [1] in both form and materiality while shielding residences from the glare of headlights on Bryant and the Freeway.

In response to Urban Design Guidelines: S3, S5, A1, A2, A3, A4, A5, A6

Bryant Street elevation highlighting the 3 vertical masses and typical 3-high bay pairings



DESIGN GUIDELINE RESPONSES

Housing Sustainability District Application Drawings

555-585 Bryant Street

San Francisco, CA 94107

05-27-2021

A - 060

DESIGNING FOR PEDESTRIAN CONTINUITY

The site is bounded by heavy traffic on Bryant Street to the north and Welsh Street to the south. Further south, across Welsh, is an existing residential building [1] that extends the entire block length between 4th and Zoe Streets, eliminating the benefit of any future mid-block pathway.

Existing street parking on Bryant Street will be replaced with a wider sidewalk and urban amenities including bike racks, seating, lighting, and street trees [2]. Ground floor PDR, double-height Residential Lobby, and Cafe facades are setback three feet from the property line to provide additional landscaping and enhanced visual connectivity between pedestrians and interior uses.

A single combined loading and parking entry on Bryant uses a new curb cut while eight existing curb cuts will be abandoned. The sidewalk at Welsh Street frontage is being widened to eliminate street parking where ground floor dwelling units with landscaped entry stoops [3] occupy the entire frontage, reducing and calming traffic to create a distinctly residential 'slow street' experience.

The material palette of the project references the surrounding neighborhood and features large format windows, metal panel cladding including metal clad bay windows [1] and detailing, and some wood and masonry elements at the ground floor along both streets.

In response to Urban Design Guidelines: S1, S5, S7, A3, A7, A8, P1, P2, P4, P5, P6, P7



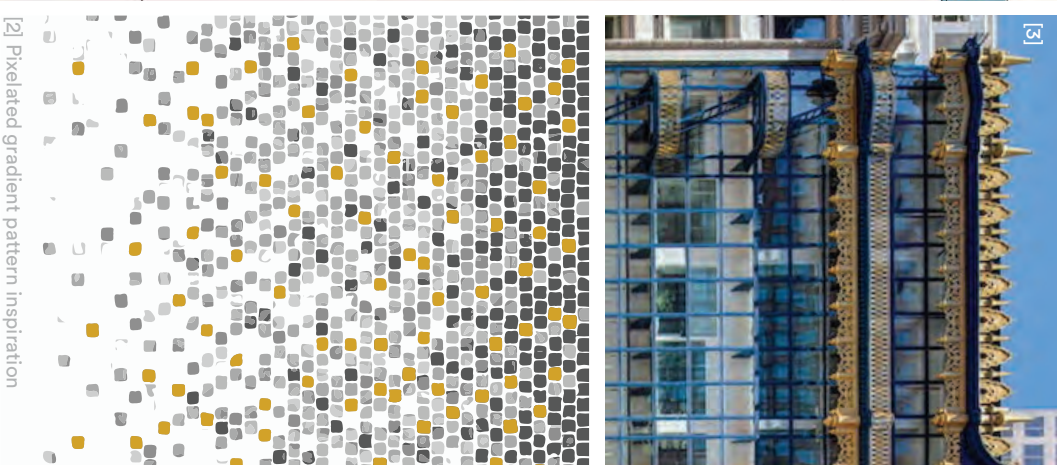
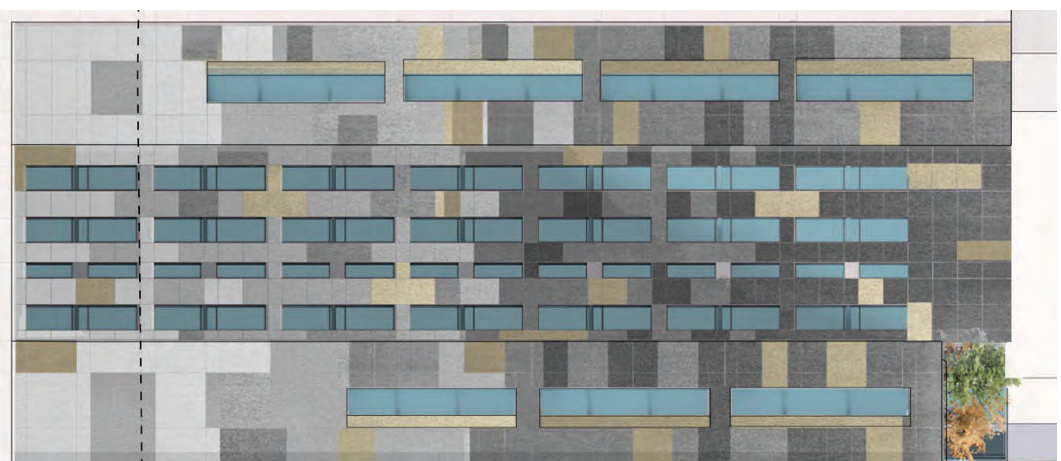
Rendering of Welsh St sidewalk (above) and illustrative landscape plan of Bryant St sidewalk (below)

URBAN GEMS, RIGHT AROUND THE CORNER

The proposed project will appear distinctly different from several vantage points and while traveling past on the freeway or along Bryant Street. While the exciting rhythm of the bays facing Bryant uses angles, offsets, and varying solidity to create a feeling of transformation & movement, the west and east facing elevations sometimes visible from the public realm [1] celebrate the revealing of internal facing building elements and enhance the overall urban composition and dynamic character. Internal facing residential courts are clad in an effervescent pattern [2] that graduates from dark to light and provides rich visual interest both from the public realm but also from individual dwelling units within the project. To further enhance this affect, the design drew influences from other noteworthy pieces of architecture in the city [3], feeding back hints of materiality, color and texture that enhanced the overall composition.

In response to Urban Design Guidelines: S2, S3, A4, A6

Glimpses of interior elevations (below) and side elevation with abstracted pattern (right)



[2] Pixelated gradient pattern inspiration

DESIGN GUIDELINE RESPONSES

Housing Sustainability District Application Drawings

555-585 Bryant Street

San Francisco, CA 94107

05-27-2021

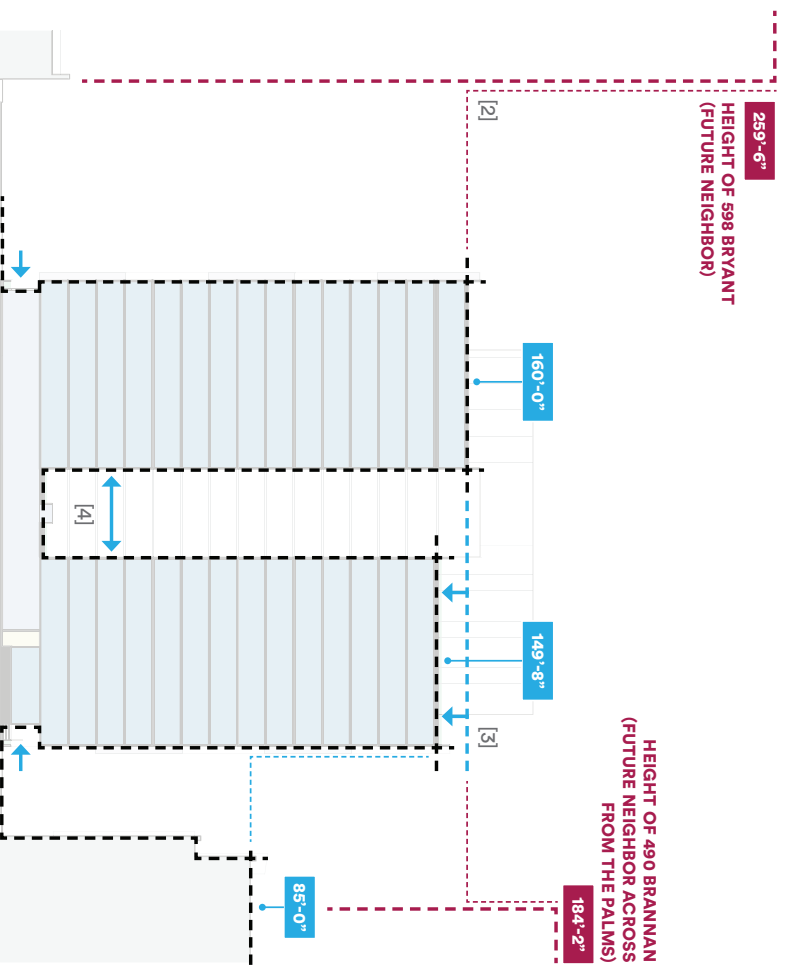
A - 062

LOOKING INWARD, LOOKING OUTWARD

This area of Central SoMa has many proposed high rise buildings, most of which will be significantly taller than the proposed project [2]. Overall height and massing are modulated to provide a step down on the south side facing the narrower of the two frontage streets and a lower residential building to the south [3].

The south facing roof terrace offers great solar access and expansive views of the southern half of the City and the Bay [1], while the use of bay windows help frame views from units of Downtown while mitigating privacy issues across narrow Welsh Street to the south. Two inner courtyards will provide a visual break at the interior lot lines, and significant sustainably landscaped areas well sheltered from the wind [4]. The eastern facing inner court will nominally align with a shallow court in the adjacent office building.

In response to Urban Design Guidelines: S2, S4, S7, S8, P7



DESIGN GUIDELINE RESPONSES

Housing Sustainability District Application Drawings
555-585 Bryant Street
San Francisco, CA 94107

05-27-2021

A - 063

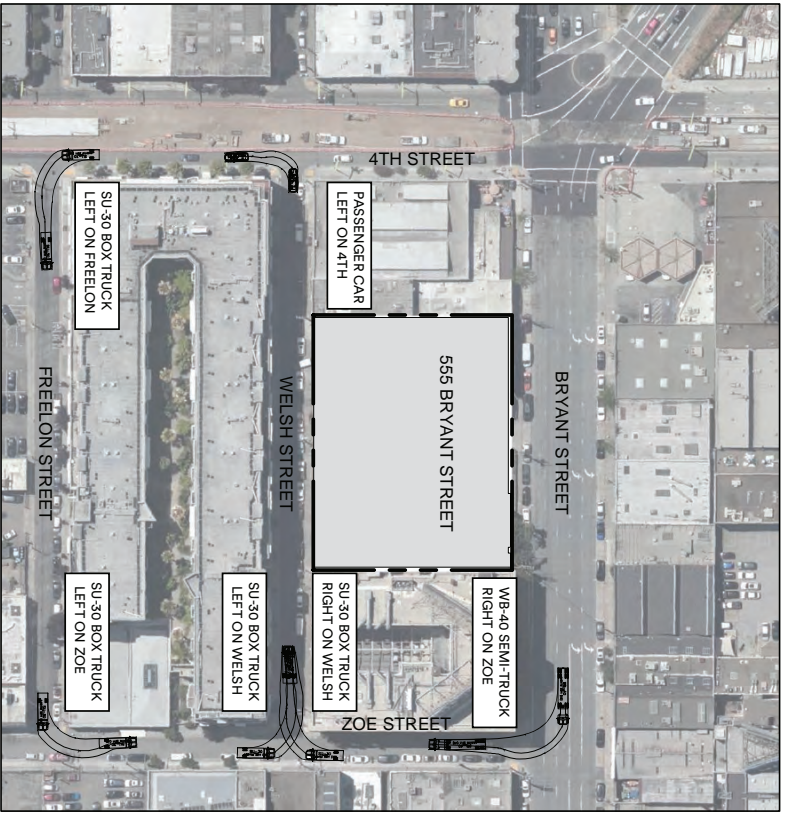


FIG. 1 LARGEST VEHICLES WITHOUT CONFLICTING TURNING MOVEMENTS

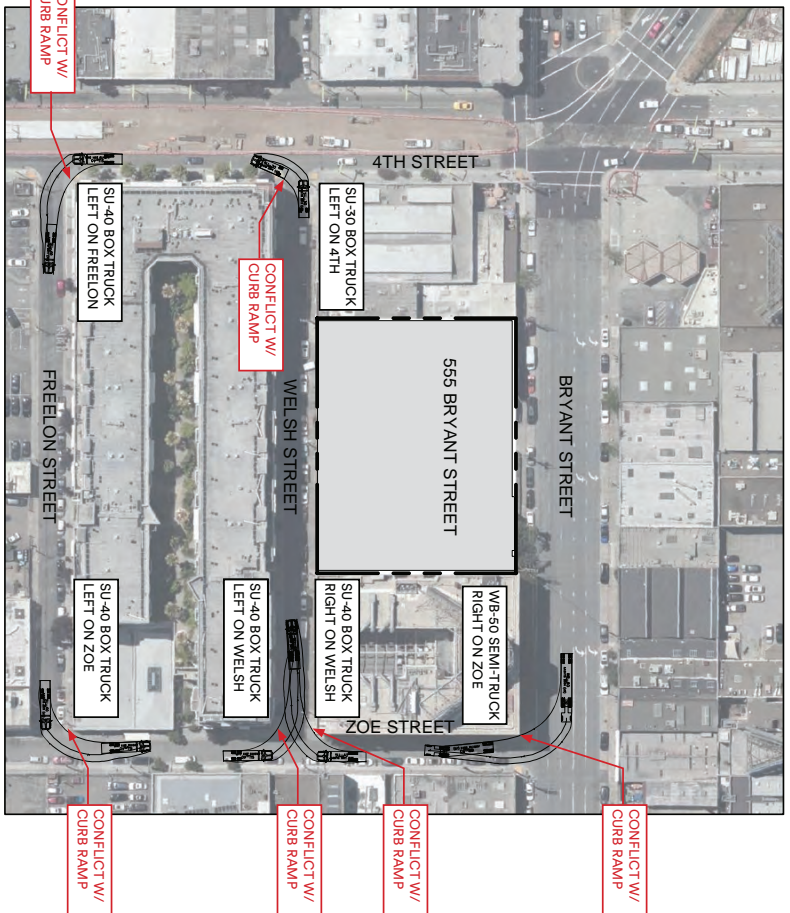
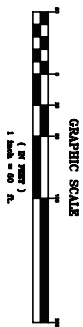


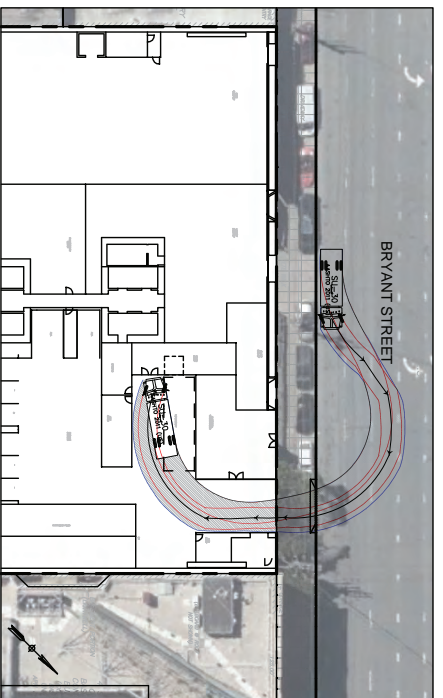
FIG. 2 SMALLEST VEHICLES WITH CONFLICTING TURNING MOVEMENTS



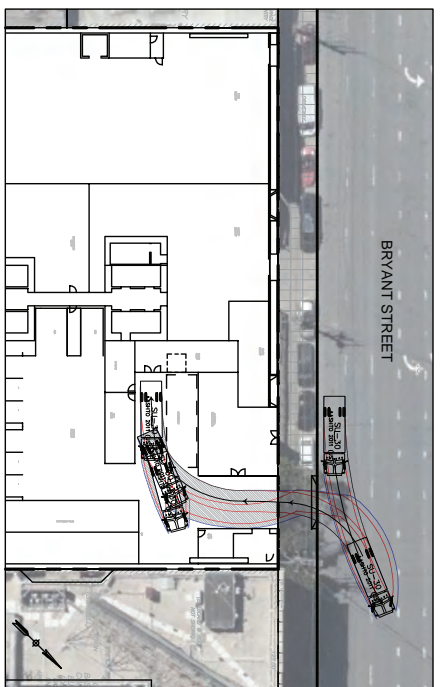
TRUCK TURNING DIAGRAMS

THE PROJECT PROPOSES TO ABANDON ALL EXISTING CURB CUTS ON BRYANT AND WELSH. A SINGLE NEW CURB CUT IS PROPOSED ON BRYANT FOR A SINGLE ENTRY SERVING BOTH SERVICE AND PASSENGER VEHICLES. BRYANT IS A MAJOR THOROUGHFARE WHILE BOTH ZOE AND WELSH ARE NARROW AND NOT WELL SUITED TO ACCOMMODATE AN INCREASE IN TRAFFIC. THE PROJECT PROPOSES TO USE ITS ENTIRE FRONTAGE AT WELSH FOR RESIDENTIAL UNITS WITH STOPS AND LANDSCAPING TO CREATE A RESIDENTIAL LANEWAY FEELING FOR WHICH ADDITIONAL VEHICLE TRAFFIC WOULD NOT BE COMPATIBLE. WELSH IS ONE-WAY, REQUIRING VEHICLES EXITING WELSH TO TURN LEFT INTO 4TH STREET WHERE THE NEW MUNI LINE FORCES A VERY TIGHT TURNING RADIUS THAT IS NOT COMPATIBLE FOR SERVICE VEHICLES AS SHOWN IN THE DIAGRAMS ABOVE.

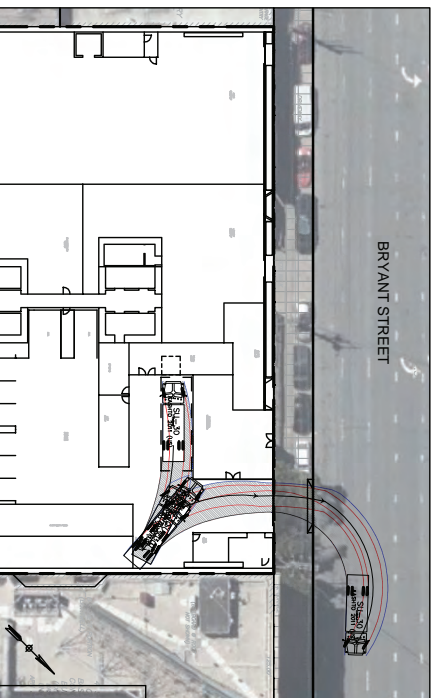
FURTHERMORE, SDAI HAS REQUESTED THAT THE PROJECT WIDEN THE SIDEWALK AND PROVIDE 100FT OF PASSENGER LOADING ZONES AT BRYANT WHICH WILL ALLOW FOR MANEUVERING INTO THE VEHICLE ENTRY ON BRYANT WITHOUT ANY ADDITIONAL DISRUPTION OF TRAFFIC OR CHANGE OF CHARACTER TO THE STREET.



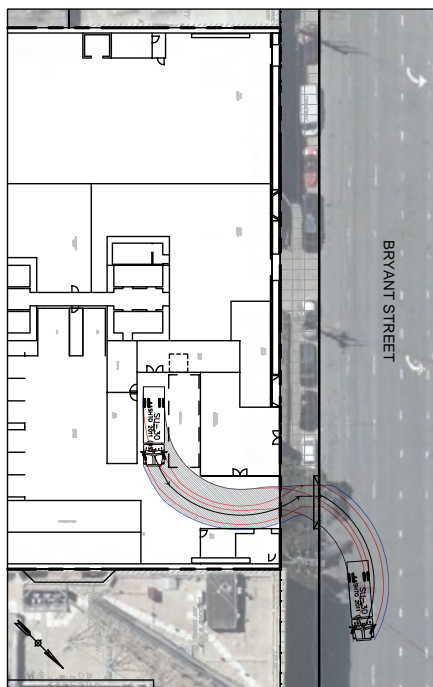
SU-30 BOX TRUCK ENTERING
FORWARD INTO BLDG, FORWARD INTO STALL



SU-30 BOX TRUCK ENTERING
BACK INTO BLDG, BACK INTO STALL



SU-30 BOX TRUCK EXITING
BACK OUT OF STALL, FORWARD EXIT



SU-30 BOX TRUCK EXITING
FORWARD OUT OF STALL, FORWARD EXIT

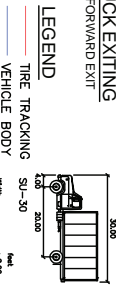
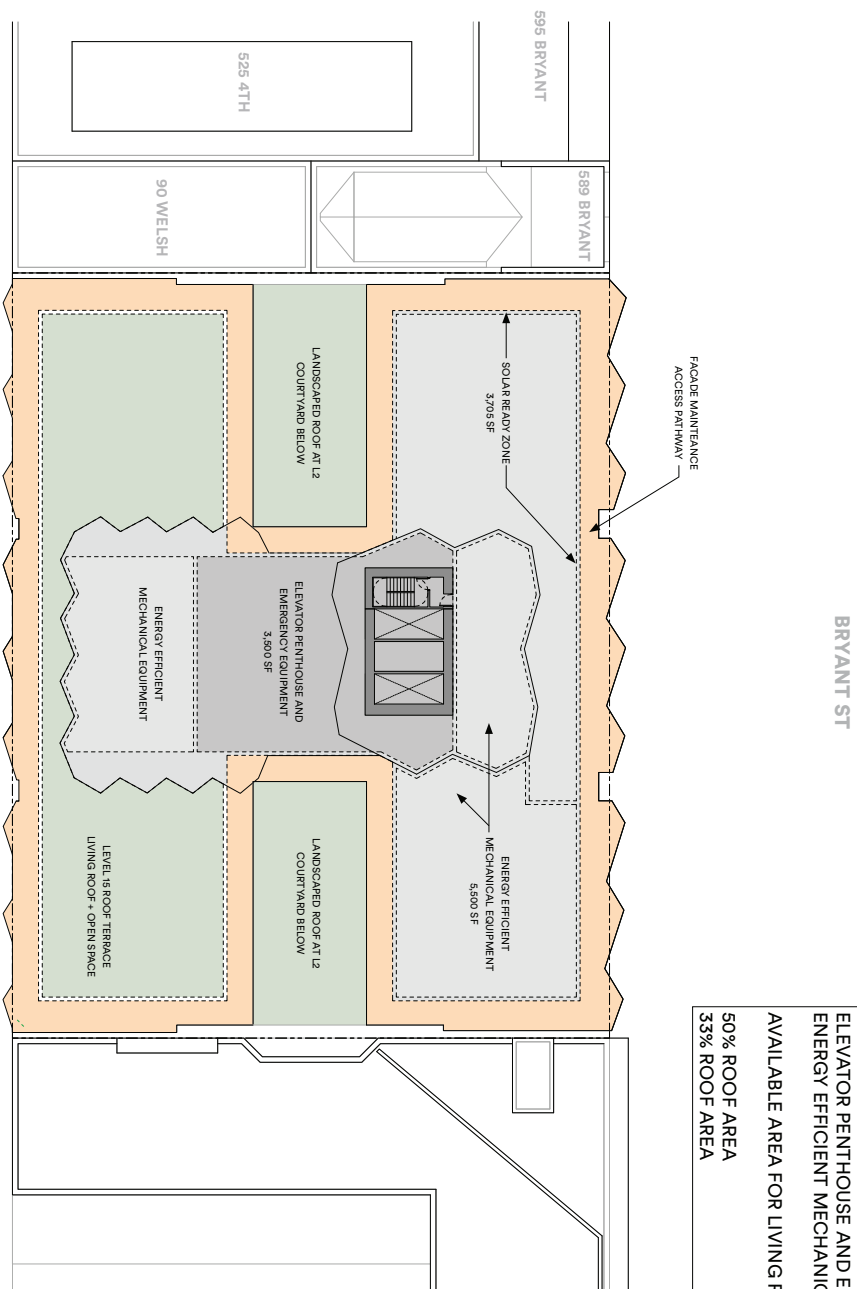


FIGURE 2 - BOX TRUCK MOVEMENTS

TRUCK TURNING DIAGRAMS
EXHIBITS FROM DRIVEWAY AND LOADING
OPERATIONS PLAN (DLOP)

ROOF AREA SUMMARY:	
ROOF AREA EXCLUDING MAINTENANCE ACCESS WALKWAY	24,705 SF
SOLAR READY ZONE	3,705 SF
ELEVATOR PENTHOUSE AND EMERGENCY EQUIPMENT	3,500 SF
ENERGY EFFICIENT MECHANICAL EQUIPMENT	5,500 SF
AVAILABLE AREA FOR LIVING ROOF AND OPEN SPACE	12,000SF
50% ROOF AREA	12,353 SF
33% ROOF AREA	8,235 SF



ROOF PLAN

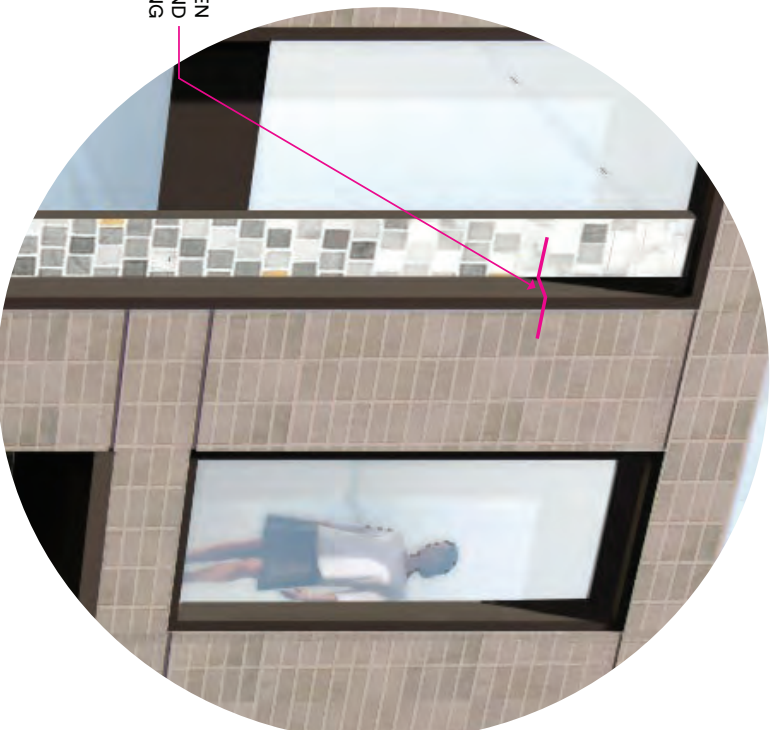
THE PROJECT REQUESTS A ZONING ADMINISTRATOR WAIVER TO REDUCE THE LIVING ROOF REQUIREMENT TO 33% OF THE ROOF AREA. WHILE THE ROOF TERRACE AT L16 WILL BE HEAVILY LANDSCAPED IT IS ALSO THE PRIMARY OPEN SPACE FOR THE PROJECT. AFTER DEDUCTIONS ARE MADE FOR NECESSARY ITEMS THAT ARE INCOMPATIBLE WITH LIVING ROOF SUCH AS ROOF MAINTENANCE PATHWAYS, SOLAR READY ZONE, ELEVATOR AND EMERGENCY EQUIPMENT, AND ENERGY EFFICIENT MECHANICAL EQUIPMENT THE AMOUNT OF ROOF AREA LEFT OVER IS ONLY 49% AND MUST BE SHARED BETWEEN LIVING ROOF AND OPEN SPACE. IF THE LIVING ROOF AREA REQUIREMENT IS REDUCED TO 33% THEN A MODEST AMOUNT OF USABLE OPEN SPACE CAN BE PROVIDED FOR THE PROJECT.

PLEASE NOTE THAT PROPOSED FOOTPRINT SCREENING AROUND ELEVATOR PENTHOUSE AND MECHANICAL EQUIPMENT IS 7,105 SQFT (25.5%) OF OVERALL ROOF AREA IN COMPLIANCE WITH 20%-30% RANGE ALLOWED BY 260(B).



5" DIFFERENCE BETWEEN RECESSED GLAZING AND FACE OF METAL PANEL

ENLARGED DETAIL AT BRYANT STREET FACADE

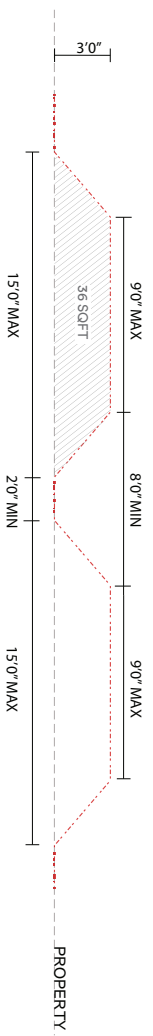


3" DIFFERENCE BETWEEN RECESSED GLAZING AND FACE OF MASONRY CLADDING

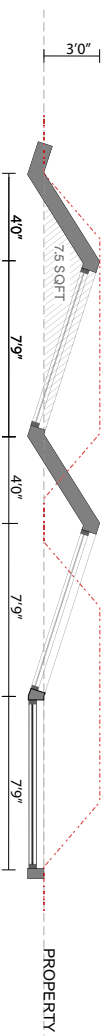
ENLARGED DETAIL AT WELSH STREET FACADE

RECESSED WINDOWS

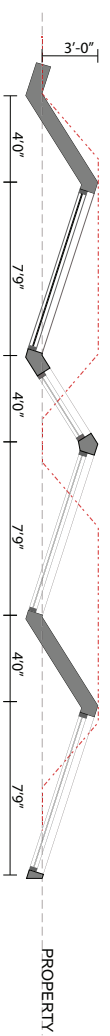
ALL OF THE WINDOWS WILL BE RECESSED FROM THE FACE OF ADJACENT FACADES TO PROVIDE ADDITIONAL DEPTH AND VISUAL TEXTURE.



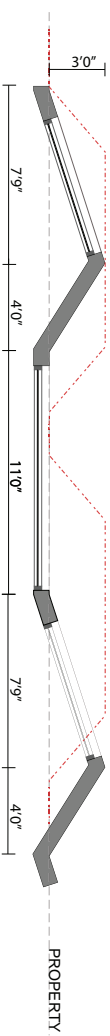
SF PLANNING - BAY WINDOW REQUIREMENTS



BRYANT ST - BAY WINDOW LAYOUT 1



BRYANT ST - BAY WINDOW LAYOUT 2



WELSH ST - BAY WINDOW LAYOUT

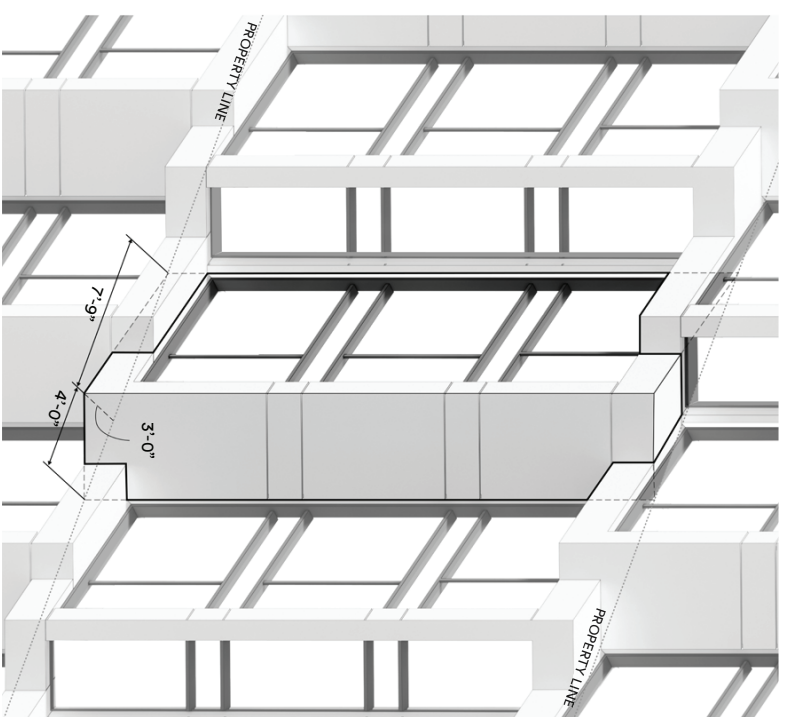
*ORIENTATION REVERSED AT LEVEL 10-15

BAY WINDOW DIAGRAMS

PROPOSED BAY WINDOWS REFERENCE THE CHARACTER OF SAN FRANCISCO'S ICONIC TRADITIONAL SIGNATURE IN A MODERN AESTHETIC SUITABLE FOR A HIGH-RISE BUILDING.

GROUPS OF ANGLED BAYS ARE COMPOSED IN AN UNDUJATING RHYTHM THAT PROVIDE TEXTURE AND A SENSE OF VISUAL MOVEMENT WHILE FRAMING VIEWS, AND SHIELDING RESIDENCES FROM THE GLARE OF HEADLIGHTS ON BRYANT AND THE FREEWAY.

WHILE THE DIMENSIONS DO NOT CONFORM TO SECTION 136, THE OVERALL AREA OF PROJECTION OVER THE PROPERTY LINE IS SIMILAR AND THE EFFECT IS BENEFICIAL TO THE PROJECT'S AESTHETIC IMPACT.



PLANNING REVIEW RESPONSES

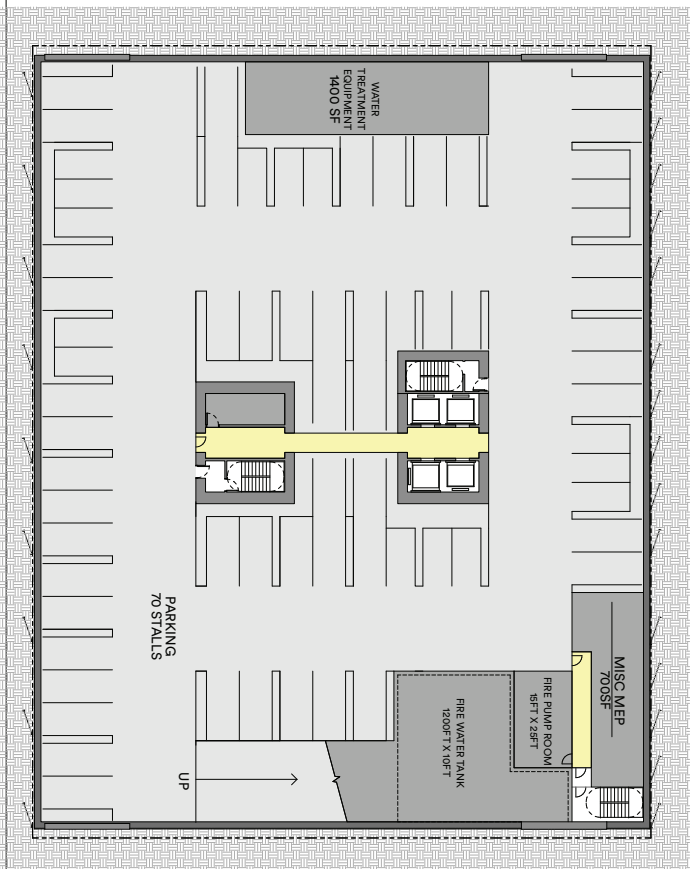
Housing Sustainability District Application Drawings
555-585 Bryant Street
San Francisco, CA 94107

05-27-2021

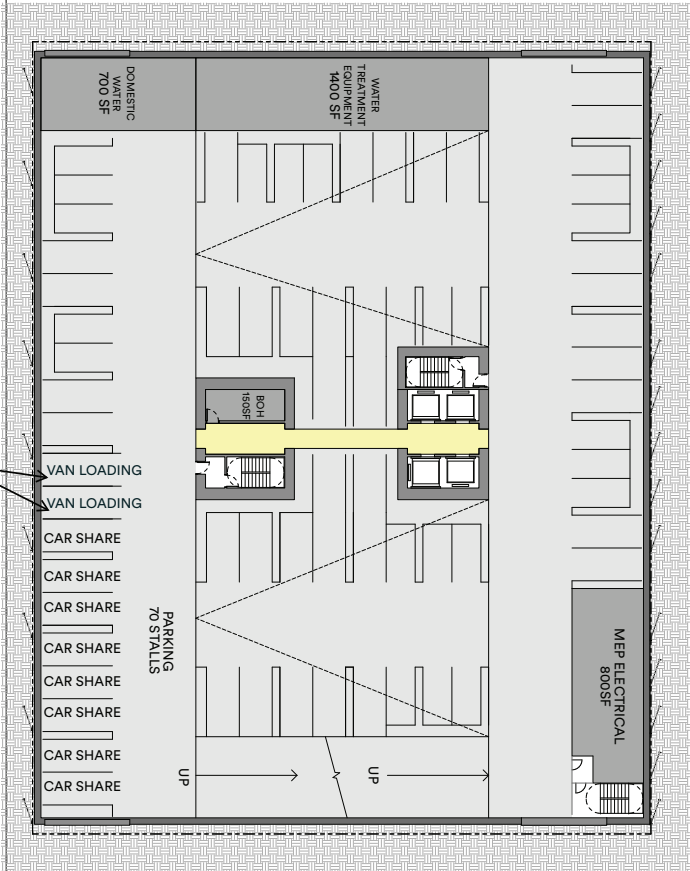
Alternate Scheme

PER PROPOSITION X AND 202.8(A)(2), PROJECTS WITH EXISTING PDR ON SITE ARE REQUIRED TO PROVIDE A MINIMUM REPLACEMENT OF 75% OF EXISTING PDR IN NEW PROJECT. 202.8(F)(6) ALLOWS NEW PROJECTS TO BE EXEMPT FROM PDR REPLACEMENT REQUIREMENT IF PROJECT RECEIVES AFFORDABLE HOUSING CREDITS FROM SOUTH BEACH MARINA APARTMENTS (BS RESOLUTION 197-16). THE PROJECT SPONSOR IS INVESTIGATING THE USE OF THESE CREDITS.

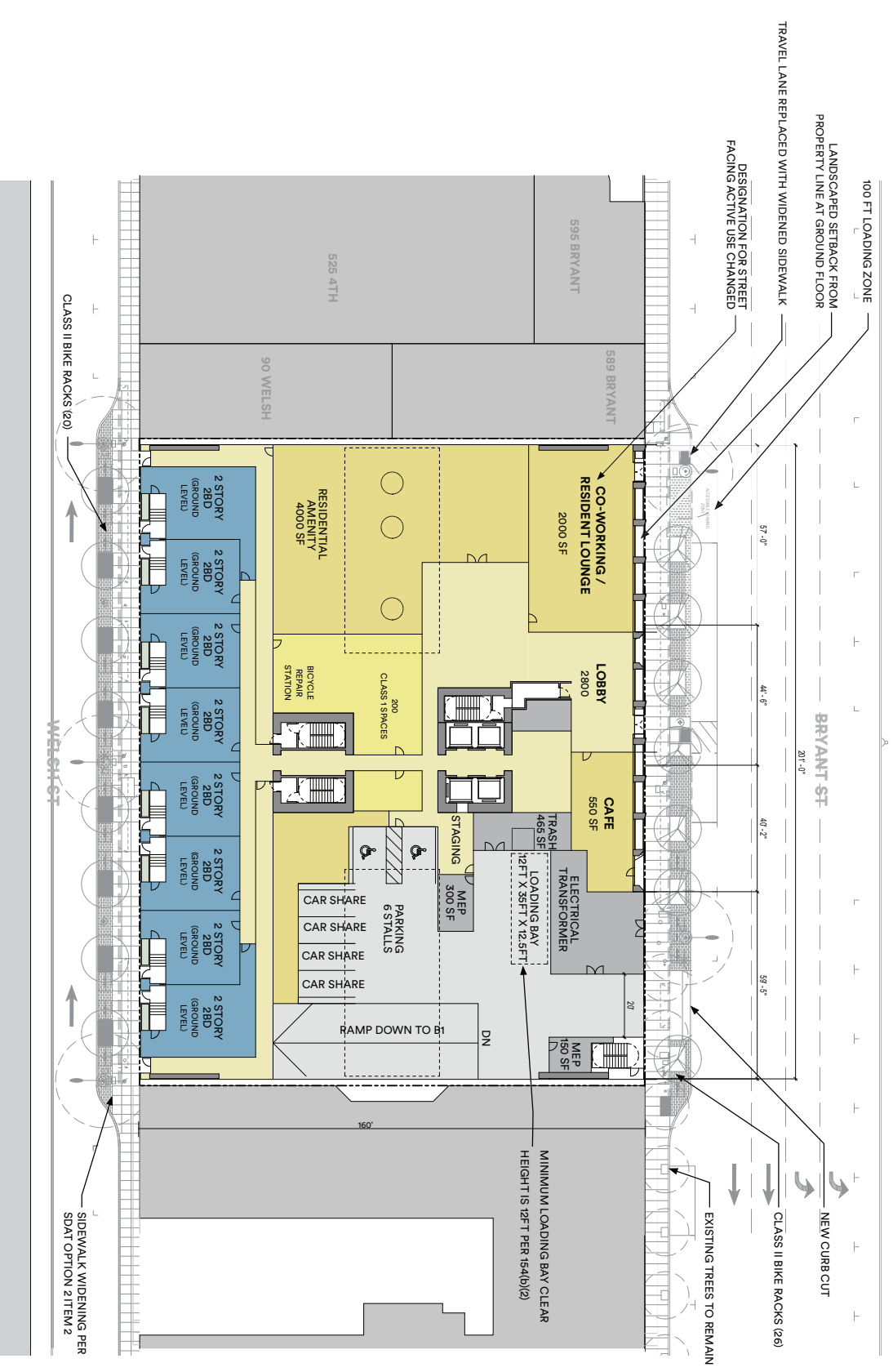
SHOWN ON THIS AND THE FOLLOWING 2 PAGES ARE ALTERNATE PLANS TO ILLUSTRATE HOW ELIMINATING PDR WOULD REDUCE THE OVERALL EXCAVATION REQUIREMENT QUANTITIES, INCREASE THE GROSS RESIDENTIAL AMENITY AREA FOR THE PROJECT, AND ALLOW MORE FLEXIBILITY IN BOTH RESIDENTIAL AND COMMUNITY AMENITY OFFERINGS.



Fire Pump is not required to be diesel powered and will be electrically powered.



SERVICE VEHICLE SPACES
 SUBSTITUTED FOR OFF-STREET
 LOADING SPACES MEASURE 8' WIDE
 x 20' LONG x 7' HIGH MINIMUM EACH



555-585 Bryant STATE DENSITY BONUS PROPOSED DESIGN AREA TABULATION

TOTAL SITE AREA*: 32,800 SF

FLR. ELEV. F/F	RESIDENTIAL UNITS/FLR.					ST	RESIDENTIAL GROSS GSF					PDR GSF	PARKING + LOADING SPACES	TOTALS GSF	
	ST	U1B	1B	2B	2B TH		400-460	500-675	660-715	835-975+	1340-1685				% Lot Coverage
+160.0															
+149.7	10.33	12	0	0	0	12	18,850	57%					4,000		
+139.0	10.67	35	6	10	10	14	28,300	86%					18,850		
+129.3	9.67	35	6	10	10	14	28,300	86%					28,300		
+119.7	9.67	35	6	10	10	14	28,300	86%					28,300		
+110.0	9.67	35	6	10	10	14	28,300	86%					28,300		
+100.3	9.67	35	6	10	10	14	28,300	86%					28,300		
+90.7	9.67	35	6	10	10	14	28,300	86%					28,300		
+81.0	9.67	35	6	10	10	14	28,300	86%					28,300		
+71.3	9.67	35	6	10	10	14	28,300	86%					28,300		
+61.7	9.67	35	6	10	10	14	28,300	86%					28,300		
+52.0	9.67	35	6	10	10	14	28,300	86%					28,300		
+42.3	9.67	35	6	10	10	14	28,300	86%					28,300		
+32.7	9.67	35	6	10	10	14	28,300	86%					28,300		
+23.0	9.67	35	6	10	10	14	28,300	86%					28,300		
+13.3	9.67	25	7	10	1	7	26,045	87%					26,045		
+0.0	13.33	8	0	0	0	0	17,525	N/A			8	10,050	30,875		
-+13.5	13.5						2,500				62	29,500	32,000		
-+25.0	11.5						2,500				55	29,500	32,000		
		500	85	140	66	201	435,320			0	125	79,751	511,670		
		TOTAL UNITS	ST 17%	U1B 28%	1B 13%	2B 40%	GSF			GSF	SPACES 0.25 per unit	GSF	GSF		

* SITE AREA MEASURED FROM ASSESSOR MAP DIMENSIONS 41.8%

SDAT Review Letter 1

April 12, 2021





San Francisco Planning

49 South Van Ness Avenue, Suite 1400
San Francisco, CA 94103
628.652.7600
www.sfplanning.org

SDAT REVIEW LETTER

Date: 4/12/2021

Project Address: 555 Bryant
Planning Record Number: 2021-00094/PRJ
Assigned Planners: Xinyu Liang, (Current Planning), Elizabeth White (Environmental Planning), Trent Greenan (Design Review), Jessica Look (SDAT)

The Street Design Advisory Team (SDAT) provides design review and guidance to projects working within the City's public right-of-way. SDAT is composed of representatives from the San Francisco Planning Department (SF Planning), the San Francisco Fire Department (Fire), San Francisco Public Works (Public Works), the San Francisco Municipal Transportation Agency (SFMTA), and the San Francisco Public Utilities Commission (SFPUC).

SDAT REVIEW HISTORY:

1st Review	2nd Review	3rd Review
11/30/20	3/22/2021	

Below are the SDAT comments from the 2nd SDAT review.

PROJECT DESCRIPTION:

The proposed Project combines 7 lots between Bryant and Welsh Streets mid-block between 4th Street and Zoe Street in the Central SoMa district. Current PDR buildings will be replaced with a 160 FT tall mixed use multi-family residential building incorporating 500 dwelling units and 115 accessory parking spaces. The project is seeking expedited approval pursuant to the Central SoMa Housing Sustainability District. The project will use the State Density Bonus to increase the size of the building allowed under current CMUO zoning but will not exceed 160 FT in height so that it qualifies for the Housing Sustainability District.

CONDITIONS REQUIRING STREET DESIGN REVIEW:

- Planning Code 138.1 (required streetscape improvements per the [Better Streets Plan](#))
- Vision Zero
- Other:

中文詢問請電 Para información en Español llámame al Para sa Impormasyon sa Tagalog tumawag sa 628.652.7550



SDAT COMMENTS

Record No. 2021-00094/PRJ

555 Bryant

SDAT COMMENTS

1. Sidewalk widening (required per Planning Code Sec. 138.1)

- As stated in previous SDAT letter, SDAT requires the project team to work with SFMTA and Public Works to develop a design for a widened sidewalk along the project's Bryant Street frontage. The existing sidewalk is 8' wide. SDAT requires widening the sidewalk to 15'. The SFMTA anticipates removing one travel lane along this block of Bryant Street to accommodate sidewalk widening. While a sidewalk widening is required in front of the project site, SDAT encourages the project to consider widening along the full block between 4th and 3rd. This would be a major community benefit to not only the future residents of the project but to the neighborhood as a whole. In addition, it would create a more unified streetscape for the block. SFMTA staff would work with project team on future design. Contact Daniel.Sheete@sfmta.com and Dustin.White@sfmta.com to develop design.
- If sidewalk widening does not extend for the full block, curb returns shall conform to SF Public Works' Standard Plan for Curb Bulbs. See <http://sfpublicworks.org/sites/default/files/87%2C175.pdf>

2. Street Trees and Welsh Street

- SDAT requires the sponsor to consider the following options for Welsh Street improvements
 - Option 1: Since the legislated sidewalk width on Welsh Street is 7', there would need to be a 6" building setback to allow for a 4' pedestrian throughway zone and trees (assuming minimum 3' tree wells). Due to the narrow sidewalk width along Welsh and required minimum dimension needed for ADA clearance, street trees cannot be planted along Welsh without this setback. Please consider this request to add to the City's urban forest and for an attractive and green building frontage and streetscape.
 - Option 2: SDAT supports widening the sidewalk to a total of 14', by removing on-street parking along the project's Welsh Street frontage. The project shall ensure a 14' clear width for fire and emergency access along Welsh Street.
 - Note that per Public Works code, bay windows cannot exceed 25% of the public sidewalk width. Without a sidewalk widening, the amount of projection allowed for bay windows over the Welsh Street sidewalk may be limited.

Follow-up for Bryant and Welsh Street Improvements
Pre-entitlement/Next SDAT
Sponsor to submit written statement to Planning expressing intention to follow-up on this item

- submit plans that differentiate existing trees from new trees
- Sponsor to work with SFMTA team to develop design for Bryant Street sidewalk and submit revised plans to SDAT showing required sidewalk widening

Post-entitlement

- Sponsor to obtain any required permits from Public Works Bureau of Urban Forestry
- Obtain relevant permits from BSM www.sfdph.org/dph/ceqa/permits
- Public Works Bureau of Urban Forestry, urbanforestry@sf.gov; (415) 554-6700

Contacts SFMTA, Daniel.Sheete@sfmta.com and Dustin.White@sfmta.com

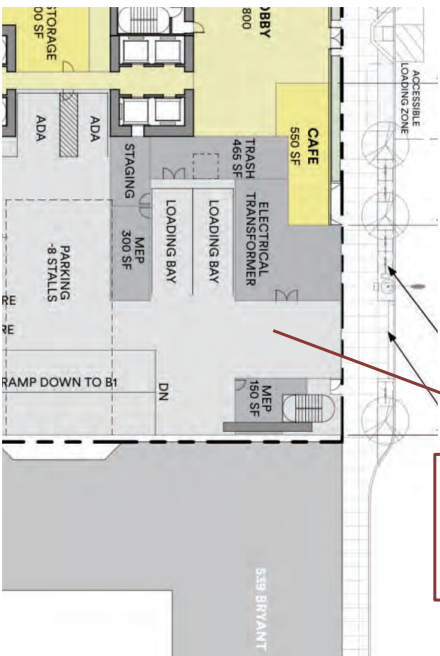
3. Off-Street Freight Loading (Requested)

- Please submit a loading operators plan describing the anticipated volume of delivery trucks, time of day deliveries that are likely to occur and size of vehicles that will serve the project
- Turn Templates: With your next submission to SDAT, please submit the following turn templates for the garage freight loading. Note that turn templates must be approved by SDAT prior to Planning entitlement.
 - o Service vehicles that will access the garage (e.g. AASHTO 2011.SU.30)

4. Driveways & Garage Access

- SDAT does support allowing off-street vehicle access from the Bryant Street curb cut to serve the building but will need to have turn templates to justify the width.
- Please note that per Planning Code Section 155(f)(3)(L) which regulates new curb cuts on protected street frontages, the project will need to seek an incentive under State Density Bonus to add a curb cut along the project's Bryant Street frontage (the Planning Department does not recognize grandfathering for existing curb cuts).

Follow-up for curb cuts, off-street parking and loading	<ul style="list-style-type: none"> • Pre-entitlement/Next SDAT Submittal • Include revised plans showing reduced curb cut width • submit turn templates
Contacts	Coordinate with your assigned Planner



5. On-street Loading (Requested)

- Due to the substantial number of residential units, it is likely the project will generate substantial demand for ride-share and/or taxi services. Residents using these services would benefit from having a dedicated passenger loading zone where they can safely enter/exit vehicles and where vehicles can stop without blocking traffic. Additionally, residents in the building may rely on paratransit services which need an accessible passenger loading zone to drop off and pick up passengers. For these reasons, SDAT continues to request a minimum 100' passenger loading zone (including accessible passenger loading) along the Bryant Street frontage.
- The project sponsor is requesting that the project be reviewed pursuant to the procedures set forth in Planning Code Section 34.3: Central SOMA Housing Sustainability District. A TIS is not required, but Planning will coordinate the review of this project with SFMTA.

Follow-up	<ul style="list-style-type: none"> • Pre-entitlement/Next SDAT • Sponsor to submit written statement to Planning expressing intention to follow-up on this item • If SDAT does request Accessible Passenger Loading, sponsor to schedule a meeting with both SFMTA Loading Team & Public Works Accessibility Coordinator to coordinate design of loading zones).
Contacts	<ul style="list-style-type: none"> • Post-entitlement (Post-Certificate of Occupancy) • Sponsor to apply for on-street loading zones from the SFMTA permits from SFMTA <p>Karna Lajere (KarnaLajere@sfdraw.org), Associate Engineer with the Public Works Disability Access Coordinator Paul Krishna (paul.krishna@sfmta.com), SFMTA Color Curb Program Manager</p>

6. Transformer

- The submitted plans show a transformer room along the Bryant frontage and indicate that the entry to the transformer room is from the garage driveway, not from the public sidewalk. Please coordinate with SFPUC or PG&E to ensure the proposed transformer room meets relevant standards.

Follow-up	<ul style="list-style-type: none"> • Pre-entitlement/Next SDAT • submit a written statement that confirms the proposed transformer location and design meets relevant SFPUC or PG&E standards.
Contacts	<ul style="list-style-type: none"> • Transformer Location (ROW, Private Property): Coordinate with your assigned Current Planner on this item • Transformer Location Technical Feasibility: Coordinate with electrical power utility (SFPUC or PG&E) and public works BSM.

ADDITIONAL INFORMATION REQUIRED FOR NEXT SDAT REVIEW

- Existing/proposed curb cuts and curb cuts to be removed
- Street names
- Dimensions of existing and proposed sidewalk and curb extensions on plans
- Dimensions of existing and proposed curb cuts on plans
- Dimensions of existing and proposed transit stops
- Site plan with streetscape features (e.g., bulbouts, trees, transit shelters, benches, bike racks)
- Proposed street tree locations
- Adjacent ROW widths
- Locations of existing utility poles and hydrants
- Turn templates for off-street loading and curb cut on Bryant Street
- Curb-to-curb section, including dimensions of tree wells and path of travel
- Proposed transformer vault location

STANDARD SDAT COMMENTS

For your next SDAT submittal, please review the "Standard SDAT Comments" which can be found on the SDAT website (<https://sfplanning.org/project-street-design-advisory-team>), and include a written statement clarifying that this task has been completed and that all plans are consistent with guidelines/standards enumerated in the "Standard SDAT Comments";

SDAT Members:

- SF Public Works:** Chris Buck, Bernane Gairne, Kevin Jensen, Jung Johnson, Karina Laird, Eric Lam, Debra Lutske, Denny Phan, Suzanne Suskind, John Thomas, Michelle Woo, John Kwong, Jennifer Cooper
- SFMTA:** Paul Krishna, Adrian Leung, Westley Myles, Francesca Napolitan, Ricardo Olea, Felipe Robles, Mike Sallaberry, Daniel Sheeter, Norman Wong, Dustin White
- SF Planning:** Ben Caldwell, Kimberly Durandet, Nicholas Foster, Ryan Shum, SeungYen Hong, Jessica Look, Ilaria Salvadori, Maia Small
- SFPUC:** Derek Adams, Mira Chokshi, Hieu Doan, Molly Petrick, Joan Ryan, Sam Young
- SFFD:** Chad Law, Gerald Gauer



Planning Code Review Checklist

March 10, 2021

LAND USE:

Permitted Use	Conditional Use	Planning Code Section & Comment
<input checked="" type="checkbox"/>	<input type="checkbox"/>	94.8 CMUO
<input type="checkbox"/>	<input type="checkbox"/>	249.78 Central SoMa SUD

Comments:

OTHER REQUIRED APPROVALS:

Required	Planning Code Section
<input checked="" type="checkbox"/>	206 Affordable Housing Bonus Programs
<input checked="" type="checkbox"/>	311 Neighborhood Notification, Change in Use from One Land Use Category
<input checked="" type="checkbox"/>	314 Residential, Hotel or Motel Uses near Places of Entertainment
<input checked="" type="checkbox"/>	34.3 Central SoMa Housing Sustainability District.

Comments:
The project is within 300 feet of a Place of Entertainment, which may be subject to an Entertainment Commission outreach process. Please reach out directly to Maggie Weiland, Executive Director, SF Entertainment Commission, maggie.weiland@sfgov.org.

ADDITIONAL PLANNING CODE REQUIREMENTS:

Complies	Does Not Comply	Needs Info	Planning Code Section	Comments
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	102 Gross Floor Area/Use Size	For base project, per DB No. 6, Sub-grade residential floor area will not be counted as residential floor area in the base density study or bonus project for the purposes of calculating the maximum residential floor area. This interpretation is narrowly applied to the calculation of maximum residential density for the purpose of implementing the State Law and does not apply to the calculation of Gross Floor Area for other purposes, including PAV and Impact Fee assessments. (July 2019)
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	1.24 & 278.49(d)(3) Floor Area Ratio	Does not apply in Central SoMa SUD.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	132.4 Setback, Street Wall and Tower Separation	Along all street- and alley-facing property lines, a 15-foot setback is required for the Mid-Rise Portion for at least 60 percent of the frontage length. Along all interior property lines, a 15-foot setback is required for the Mid-Rise Portion for the entire frontage.
				The project seeks a waiver under SDB.

Complies	Does Not Comply	Needs Info	Planning Code Section	Comments
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	135 Open Space	80 sq. ft. per unit; 54 sq. ft. per unit if publicly accessible.
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	135.3 Open Space (Non-Residential)	The project seeks an Incentive under SDB. (this will be considered as an incentive, not a waiver, as the alternative is the in-lieu fee per EC 4.27). Please revised the SDB supplemental application.
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	136 Permitted Obstructions	Please identify a PDR use. Open space requirement varies by the type of PDR uses proposed. None required for Manufacturing and Light Industrial use.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	138 POPoS	Please provide a diagram showing how the proposed bay window meets the requirement. Not Applicable. In the Central SoMa SUD, it applies to project proposing new construction or addition of 50,000 sqft or more of Non-Residential use.
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	138.1 Streetscape Plan	The project will be reviewed by SPAT in the next submittal. Comments to be provided.
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	139 Bird Safety	The project will be reviewed by SPAT in the next submittal. Comments to be provided.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	140 & 249.78(d)(1) Dwelling Unit Exposure	The requirements of Section 140 shall apply, except that the required windows of at least one room that meets the 120-square-foot minimum superficial floor area requirement shall face on an open area as follows: (A) For units constructed above 85 feet in height, the required windows shall face directly on an open area that is no less than 15 feet by 15 feet; (B) 10% of units constructed at or below 85 feet may face directly onto an open area that is no less than 15 feet by 15 feet; and (C) Where required windows are built on an open area pursuant to 140(A)(2), the requirements to increase the horizontal dimension at each subsequent floor do not apply. In this case, a minimum of 25' by 25' courtyard is required to be considered as a code compliant open area for exposure. The project seeks a waiver under SDB.
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	141 Rooftop Screening	
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	142 Parking Screening and Greening	
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	145.1(c)(1) Above-grade Parking Setback	

Complies	Does Not Comply	Needs Info	Planning Code Section	Comments	
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	145.1(c)(2)	Planning Code Section Parking and Loading Entrances	No more than one-third of the width or 20 feet, whichever is less, of any given street frontage of a new or altered structure parallel to and facing a street shall be devoted to parking and loading ingress or egress. Please provide dimensions for the proposed parking/loading entrance.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	145.1(c)(3) & 249.78(c)	Required Active Use	Building lobbies are considered active uses, so long as they do not exceed 40 feet or 25 percent of building frontage, whichever is larger. Please provide the dimension of the lobby on both proposed and alternative plans
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	145.1(c)(4) & 249.78(d)(10)	Ground Floor Ceiling Height	Flex amenity space; please provide additional information on the space. Spaces accessory to residential uses, such as fitness or community rooms, are considered active uses only, if they meet the intent of this section and have access directly to the public sidewalk or street. For base project without PDR version: Ground floor Non-Residential Uses in CMUD District shall have a minimum floor-to-floor height of 14 feet, as measured from grade. For base project with PDR version: PDR space that is subject to the requirements of Section 202.8 or 249.78(c)(5) shall have a minimum floor-to-floor height of 17 feet, as measured from grade. Separately, for base project, per <u>DBM</u> , the Base Density study must assume a floor to floor height of 9 feet, except that the ground floor ceiling height must comply with requirements set forth in Planning Code Section 145 and 249.78. Please provide a section demonstrating compliance in the base project. Mezzanine level on the ground floor may be helpful to provide additional square footage needed in the base project.
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	145.1(c)(5)	Street-Facing Ground-Level Spaces	The project seeks a waiver under SDB since the height for HSD is capped at 180.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	145.1(c)(6)	Transparency & Fenestration	PDR active uses greater than 50 feet shall require transparent windows and doors for 30% of the frontage.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	145.1(c)(7)	Gates, Railings and Grillwork	

Complies	Does Not Comply	Needs Info	Planning Code Section	Comments	
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	147	Planning Code Section Shadow on Certain Public Open Space	Shadow analysis is required. Please work with EP.
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	149 & 249.78(d)(4)	Living and Solar Roofs and Living Walls	New Construction: at least fifty percent of the total roof area shall be covered by one or more Living Roofs. Show area for photovoltaic panels. Vertical living walls on building facades are encouraged
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	151.1	Off-street Parking	Please demonstrate compliance as this is a Building code requirement and an Incentive/concession is not applicable as this is a Building Code requirement. Permitted up to 125 parking spaces. P up to one car for each four Dwelling Units.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	152.1	Off-street Freight Loading	2 Off-Street Loading Spaces are required for Residential OFA between 200k – 500k sq. Please show them with dimensions on both proposed and alternative plans.
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	154	Parking Dimensions	1 PDR loading is required for majority of PDR uses. Please show 1 loading with dimensions on proposed plans. Every required off-street freight loading space shall have a minimum length of 35 feet, a minimum width of 12 feet, and a minimum vertical clearance including entry and exit of 14 feet, except as provided in the code.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	155(f)	Protected Street Frontages (Curb Cuts)	Curb cuts are not permitted as this portion of Bryant St is considered a Transit Preferential St per PC 155(f)(3). The project seeks an incentive/concession under SDB.
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	155(i)	Driveway and Loading Operations Plan (DLOP)	Required for Development projects of over 100k sq ft in Central SoMa SUD. Please work with EP.

Complies	Does Not Comply	Needs Info	Planning Code Section	Comments
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	155.2 Bicycle Parking	Residential: Class 1: One Class 1 space for every Dwelling Unit. For buildings containing more than 100 Dwelling Units, 100 Class 1 spaces plus one Class 1 space for every four Dwelling Units over 100. 201 Class 1 spaces are required. Class 2: One per 20 units. 25 Class 2 spaces are required. Industrial: Class 1: One Class 1 space for every 12,000 square feet of Occupied Floor Area, except not less than two Class 1 spaces for any use larger than 5,000 occupied square feet. Class 2: Minimum of two spaces. Please note the number of Class 1 bike parking on proposed and alternative plans. The number of Class 2 parking proposed is not sufficient. Does the project plan to pay the in-lieu fee?
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	155.4 Required Showers & Lockers	One shower and six clothes lockers where the Occupied Floor Area exceeds 10,000 square feet but is no greater than 20,000 square feet. Please clarify that in SDB supplemental requests a waiver for showers/lockers but on proposed plans A-041 states: "PDR INTERIOR SPACE INCLUSIVE OF LOCKERS AND SHOWERS PER 155.4 AND MEP EQUIPMENT REQUIRED BY FUTURE TENANTS".
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	163 Transportation Management Program	The property owner shall be required to provide on-site transportation brokerage services for the actual lifetime of the project. Prior to the issuance of a temporary permit of occupancy, the property owner shall execute an agreement with the Planning Department for the provision of on-site transportation brokerage services.
<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	166 Car Share	2, plus 1 for every 200 dwelling units over 200. 4 car share spaces required. Please note the location of car share spaces on proposed and alternative plans.
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	169 Transportation Demand Management	To be reviewed in the next submittal

Complies	Does Not Comply	Needs Info	Planning Code Section	Comments
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	202.8 249.78(c)(9) PDR and Community Building Space	As of July 1, 2016, the site was zoned SLU, the replacement space shall include 0.75 square foot of PDR, Institutional Community, or Arts Activities use for each square foot of the use proposed for conversion. Any project that meets the requirements of this subsection 249.78(c)(9) and the PDR replacement requirements of Section 202.8 shall not be subject to the Conditional Use Authorization required by Section 202.8. 202.8 shall be exempt for project that receives affordable housing credits associated with retention of affordable units at the South Beach Marina Apartments, pursuant to Board of Supervisors Resolution No. 197-16. For base project, if the project is seeking South Beach Marina Credit, no PDR is required in the base project. Thus, the minimum floor-to-floor height shall be no less than 14 feet (as opposed to 17 if PDR is provided) in the no PDR version of the base project.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	249.78(d)(1) Prevailing Building Height and Density	A project may exceed the Prevailing Building Height and Density Limits up to the maximum height and density otherwise permitted in this Code and the Zoning Map in cases where the project sponsor elects to develop a project subject to Section 434.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	249.78(d)(5) Renewable Energy	All new construction projects shall commit, as a condition of approval, to fulfilling all on-site electricity demands through any combination of on-site generation of 100% green-house gas-free electricity and purchase of electricity from 100% green-house gas-free sources for a period of not less than 25 years from the issuance of entitlement.

Complies	Does Not Comply	Needs Info	Planning Code Section	Comments
<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	249.78(d)(6) Lot Coverage	Lot coverage is limited to 80 percent at all residential levels, except that on levels in which all residential units face onto a public right-of-way, 100 percent lot coverage may occur. For base project, per DB No.6, projects in Central SOMA (Section 249.78(d)(6)) must calculate base density assuming 80% coverage on all residential levels. The base density study may not assume full lot coverage. (July 2019). Please demonstrate compliance for the base project.
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	249.78(d)(9) Wind	The project seeks a waiver under SDB1. Applies to new buildings above 85 feet in Height and additions to existing buildings that result in a building above 85 feet in Height. Please work with EP.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	260 Height	130-CS The project seeks a waiver under SDB1.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	260(b) Exemptions from Height	The project seeks a waiver under SDB1. Mechanical equipment, elevator, stair and mechanical penthouses: the exemption shall be limited to the top 16 feet.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	261.1 Narrow Streets and Alleys	walsh STs considered an East-West Narrow Street, which is subject to AMR under PC 270(h); 35' of Base Height at 85% AMR. For projects that are required to provide PDR pursuant to Sections 202.8 and 249.78(c)(5), if such PDR is provided on the ground floor or above, add 3 vertical feet to the Base Height.
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	261.32 Special Height Exceptions	The project seeks a waiver under SDB1. [allow additional 25' above the applicable height limit without a CUA if the project meets certain conditions]
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	270 (h) Apparent Mass Reduction	Bryant St (Major St); 85' Base Height at 67% AMR. The project seeks a waiver under SDB1.
<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	270.1 Horizontal Mass Reductions in Large Lots	Buildings subject to this Section must incorporate one or more mass reduction breaks in the building that reduce the horizontal scale of the building into discrete sections not more than 200 feet in length.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	270.2 Mid-Block Alleys in Large Lots	The project seeks a waiver under SDB1. For new construction on lots with frontage greater than 200 linear feet but less than 300 feet the

Complies	Does Not Comply	Needs Info	Planning Code Section	Comments
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>		project shall provide a publicly-accessible mid-block alley for the entire depth of the property. The alleys provided per subsections (a) and (b) above shall meet the following standards: (4) Have a minimum width of 20 feet, exclusive of those obstructions allowed within setbacks pursuant to Section 136. (6) Have at least 60 percent of the area of the alley or pathway open to the sky. Obstructions permitted within setbacks pursuant to section 136 may be located within the portion of the alley or pathway that is required to be open to the sky. All portions of the alley or pathway not open to the sky shall have a minimum clearance height from grade of 15 feet at all points. The project seeks a waiver under SDB1.

DEVELOPMENT IMPACT FEES:

Required	Planning Code Section
<input checked="" type="checkbox"/>	411A Transportation Sustainability Fee (TSF)
<input checked="" type="checkbox"/>	415A Child-Care for Residential Projects
<input checked="" type="checkbox"/>	415 Inclusionary/Affordable Housing Program
<input checked="" type="checkbox"/>	423 Eastern Neighborhoods Impact Fee and Public Benefits Fund
<input type="checkbox"/>	430 Bicycle Parking In-Lieu Fee
<input checked="" type="checkbox"/>	432 Central SoMa Community Services Facilities Fee and Fund
<input checked="" type="checkbox"/>	433 Central SoMa Infrastructure Impact Fee and Fund
<input checked="" type="checkbox"/>	434 Central SoMa Community Facilities District Program

Urban Design Guidelines Matrix

URBAN DESIGN GUIDELINES MATRIX			
Project address	555-585 Bryant St	REVIEW TYPE	DES R/W
Application number	2021-000947PRJ	Date of Review / Response	3/17/2021
Quadrant	SE	Date of Drawings	1/20/2021
Assigned Planner	Xinyu Liang	Comment author	Trent Greenan
Assigned Design Review staff	Trent Greenan	Meeting Attendees	Xinyu Liang, David Winslow, Allison Alberici, Luz Barata, Glenn Cabreros
#	Guideline	Site Design	
S1	Recognize and Respond to Urban Patterns	MEETS GUIDELINE	
S2	Harmonize Relationships between Buildings, Streets, and Open Spaces	MEETS GUIDELINE	
S3	Recognize and Enhance Unique Conditions	NA	
S4	Create, Protect, and Support View Corridors	NA	
S5	Create a Defined and Active Streetwall	MEETS GUIDELINE	
S6	Organize Uses to Complement the Public Environment	Provide Active Ground Floor Use in Flex Amenity Space along Bryant	
S7	Integrate Common Open Space and Landscape with Architecture	Consider introducing some balconies to provide private useable open space	
S8	Respect and Exhibit Natural Systems and Features	NA	
Architecture			
A1	Express a Clear Organizing Architectural Idea	Recommend using the same material expression of townhomes at the 3rd level to provide more solidity at the base on Wash St.	
A2	Modulate Buildings Vertically and Horizontally	MEETS GUIDELINE	
A3	Harmonize Building Designs with Neighboring Scale and Materials	Provide windows details showing recess	
A4	Design Buildings from Multiple Vantage Points	MEETS GUIDELINE	
A5	Shape the Roofs of Buildings	MEETS GUIDELINE	
A6	Render Building Facades with Texture and Depth	MEETS GUIDELINE	

555-585 Bryant St - UDGm.xlsx

Guidelines Matrix

4/6/2021

URBAN DESIGN GUIDELINES MATRIX			
Project address	555-585 Bryant St	REVIEW TYPE	DES R/W
Application number	2021-000947PRJ	Date of Review / Response	3/17/2021
Quadrant	SE	Date of Drawings	1/20/2021
Assigned Planner	Xinyu Liang	Comment author	Trent Greenan
Assigned Design Review staff	Trent Greenan	Meeting Attendees	Xinyu Liang, David Winslow, Allison Alberici, Luz Barata, Glenn Cabreros
#	Guideline	Public Realm	
A7	Coordinate Building Elements	MEETS GUIDELINE	
A8	Design Active Building Fronts	MEETS GUIDELINE	
A9	Employ Sustainable Principles and Practices in Building Design	Incorporate Better Roofs Ordinance into design	
P1	Design Public Open Spaces to Connect with and Complement the Streetscape	NA	
P2	Locate and Design Open Spaces to Maximize Physical Comfort and Visual Access	NA	
P3	Express Neighborhood Character in Open Space Designs	NA	
P4	Support Public Transportation and Bicycling	Consider reducing parking to better support bicycling and transit	
P5	Design Sidewalks to Enhance the Pedestrian Experience	Design review supports the SDAT recommendation on sidewalk widening on Bryant St.	
P6	Program Public Open Spaces to Encourage Social Activity, Play, and Rest	MEETS GUIDELINE	
P7	Integrate Sustainable Practices into the Landscape	MEETS GUIDELINE	

555-585 Bryant St - UDGm.xlsx

Guidelines Matrix

4/6/2021

Planning Code Review Checklist

March 14, 2021

LAND USE:

Permitted Use	Conditional Use	Planning Code Section & Comment
<input checked="" type="checkbox"/>	<input type="checkbox"/>	948 CMUO
<input checked="" type="checkbox"/>	<input type="checkbox"/>	249.78 Central SOMA SUD

Comments:

OTHER REQUIRED APPROVALS:

Required	Planning Code Section
<input checked="" type="checkbox"/>	206 Affordable Housing Bonus Programs
<input checked="" type="checkbox"/>	311 Neighborhood Notification, Change in Use from One Land Use Category
<input type="checkbox"/>	314 Residential, Hotel or Motel Uses near Places of Entertainment
<input checked="" type="checkbox"/>	343 Central SOMA Housing Sustainability District

Comments:

ADDITIONAL PLANNING CODE REQUIREMENTS:

Complies	Does Not Comply	Needs Info	Planning Code Section	Comments
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	102 Gross Floor Area/Use Size	
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	124 & Floor Area Ratio	
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	132.4 Setback, Street Wall and Tower Separation	The project seeks a waiver under SDB.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	135 Open Space	The project seeks a Waiver under SDB. (this will be considered as a waiver because a total of 40,000 sf of OS is required while the total available roof area is only about 24,705 sf.)
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	135.3 Open Space (Non-Residential)	
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	136 Permitted Obstructions	The project is seeking a waiver for bay windows not conforming with traditional bay windows' size/shape but providing additional residential floor area under 160' height limit. If additional wind measures proposed does not meet this code section, the project may seek a waiver under SDB.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	138 POPOS	See SDAT letter, if applicable.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	138.1 Streetscape Plan	
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	139 Bird Safety	
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	140 & Dwelling Unit Exposure	PC 248.78(d)(11): (A) For units constructed above 85 feet in height, the required windows shall face
			249.78(d)(11)	

Complies	Does Not Comply	Needs Info	Planning Code Section	Comments
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	141 Rooftop Screening	
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	142 Parking Screening and Greening	
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	145.1(c)(1) Above-grade Parking Setback	
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	145.1(c)(2) Parking and Loading Entrances	No more than one-third of the width or 20 feet, whichever is less, of any given street frontage of a new or altered structure parallel to and facing a street shall be devoted to parking and loading ingress or egress. Please provide dimensions for the proposed parking/loading entrance. The project seeks an incentive for a 22' wide entrance.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	145.1(c)(3) & 249.78(c) Required Active Use	
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	145.1(c)(4) & 249.78(d)(10) Ground Floor Ceiling Height	The project seeks a waiver under SDB since the ground floor ceiling height for HSD is capped at 160'.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	145.1(c)(5) Street-Facing Ground-Level Spaces	
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	145.1(c)(6) Transparency & Fenestration	

Complies	Does Not Comply	Needs Info	Planning Code Section	Comments	
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	145.1(c)(7)	Gates, Railings and Grillwork	
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	147	Reduction of Shadow on Certain Public Open Space	Shadow analysis is required. Please work with EP.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	149 & 249.78(d)(4)	Living and Solar Roofs and Living Walls	The project seeks an incentive under SDB.
<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	151.1	Off-street Parking	P up to one car for each four Dwelling Units. Permitted up to 125 parking spaces. How many parking spaces provided? On the plans for alternative option (Sheets A070-072), it shows 146, which is not permitted.
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	152.1 153	Off-street Freight Loading	2 Off-Street Loading Spaces are required for Residential OFA between 200k - 500k sf. In CUMCO, substitution of two service vehicle spaces for each required off-street freight loading space may be made, provided that a minimum of 50 percent of the required number of spaces are provided for freight loading, 1 off-street loading and 2 service vehicle parking provided. Please show them with dimensions on both proposed and alternative plans. 1 PDR loading is required for majority of PDR uses. Please show 1 loading with dimensions on proposed plans. The project may seek a waiver under SDB.
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	154	Parking Dimensions	Every required off-street freight loading space shall have a minimum length of 35 feet, a minimum width of 12 feet, and a minimum vertical clearance including entry and exit of 14 feet, except as provided in the code.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	155(f)	Protected Street Frontages (Curb Cuts)	The project may seek a waiver under SDB. The project seeks an incentive/concession under SDB.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	155(j)	Onway and Loading Operations Plan (DLOP)	

Complies	Does Not Comply	Needs Info	Planning Code Section	Comments	
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	155.2	Bicycle Parking	Residential: Class 1: One Class 1 space for every Dwelling Unit. For buildings containing more than 100 Dwelling Units, 100 Class 1 spaces plus one Class 1 space for every four Dwelling Units over 100. 201 Class 1 spaces are required. Class 2: One per 20 units. 25 Class 2 spaces are required.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	155.4	Required Showers & Lockers	Industrial: Class 1: One Class 1 space for every 12,000 square feet of Occupied Floor Area, except not less than two Class 1 spaces for any use larger than 5,000 occupied square feet. (not provided) Class 2: Minimum of two spaces. Please note the number of Class 1 bike parking on proposed and alternative plans.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	163	Transportation Management Program	
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	166	Car Share	
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	169	Transportation Demand Management	To be reviewed again once SDB supplemental application is submitted. (points are tie to the % of affordable housing provided) Car share Option E: one car-share parking space for every 40 provided Dwelling Units. Show adequate car share spaces on the plans Active 5a: show bike repair station on the plans Bicycle Repair Station: identify it on the plans
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	202.8	PDR and Community Building Space	
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	249.78(d)(1)	Prevailing Building Height and Density	
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	249.78(d)(5)	Renewable Energy	
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	249.78(d)(6)	Lot Coverage	The project seeks a waiver under SDB.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	249.78(d)(9)	Wind	The project may seek a waiver under SDB.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	260	Height	The project seeks a waiver under SDB.

Complies	Does Not Comply	Needs Info	Planning Code Section	Comments
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	260(b)	Exemptions from Height Mechanical equipment + stair and elevator penthouses are limited to 20%-30% of roof area pursuant to 260(b).
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	261.1	Narrow Streets and Alleys The project seeks a waiver under SDB.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	270 (h)	Apparent Mass Reduction The project seeks a waiver under SDB.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	270.1	Horizontal Mass Reductions in Large Lots The project seeks a waiver under SDB.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	270.2	Mid-Block Alleys in Large Lots The project seeks a waiver under SDB.

DEVELOPMENT IMPACT FEES:

Required	Planning Code Section
<input checked="" type="checkbox"/>	411A Transportation Sustainability Fee (TSF)
<input checked="" type="checkbox"/>	414A Child-Care for Residential Projects
<input checked="" type="checkbox"/>	415 Inclusionary/Affordable Housing Program
<input checked="" type="checkbox"/>	423 Eastern Neighborhoods Impact Fee and Public Benefits Fund
<input type="checkbox"/>	430 Bicycle Parking In-Lieu Fee
<input checked="" type="checkbox"/>	432 Central SoMa Community Services Facilities Fee and Fund
<input checked="" type="checkbox"/>	433 Central SoMa Infrastructure Impact Fee and Fund
<input checked="" type="checkbox"/>	434 Central SoMa Community Facilities District Program

SDAT Review Letter 2

May 24, 2021





San Francisco
Planning

49 South Van Ness Avenue, Suite 1400
San Francisco, CA 94103
628.682.7600
www.sfplanning.org

SDAT REVIEW LETTER

Date: 5/24/2021

Project Address: 555-585 Bryant St
Planning Record Number: 2021-00094/PRJ
Assigned Planners: Xinyu Liang (Current Planning), Elizabeth White (Environmental Planning), Trent Greenan (Design Review), Jessica Look/Seung Yen Hong (SDAT)

The Street Design Advisory Team (SDAT) provides design review and guidance to projects working within the City's public right-of-way. SDAT is composed of representatives from the San Francisco Planning Department (SF Planning), the San Francisco Fire Department (Fire), San Francisco Public Works (Public Works), the San Francisco Municipal Transportation Agency (SFMTA), and the San Francisco Public Utilities Commission (SFPUC).

SDAT REVIEW HISTORY:

1st Review	2nd Review	3rd Review
11/30/20	3/22/2021	May 10, 2021

Below are the SDAT comments from the 3rd SDAT review.

CONDITIONS REQUIRING STREET DESIGN REVIEW:

- Planning Code 138.1 (required streetscape improvements per the [Better Streets Plan](#))
- Vision Zero
- Other:

SDAT COMMENTS

1. Bryant Sidewalk Widening (required per Planning Code Sec. 138.1)

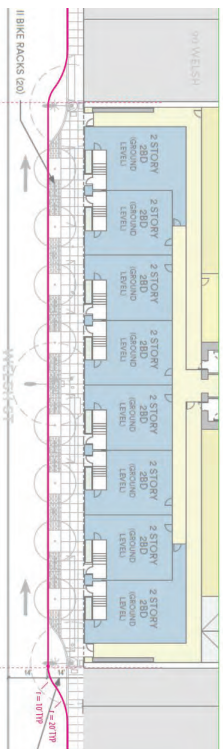
- The sponsor indicated that the project would widen the sidewalk along Bryant Street from 4th Street to Zoe, and that the sponsor would like to seek an in-kind agreement for the sidewalk improvements beyond the project property line.
- While the sponsor is seeking an in-kind agreement, SDAT recommends working with SFMTA to create an interim street improvement plan because the in-kind agreement may not be approved. Contact Daniel Sheeter@sfmta.com and Dustin.White@sfmta.com to develop design.

SDAT COMMENTS

Record No. 2021-00094/PRJ
555-585 Bryant St

2. Welsh Sidewalk Widening (required per Planning Code Sec. 138.1)

- The sponsor confirmed that the project would widen the Welsh Street sidewalk to 14'.
- Note that the sidewalk widening shall extend the entire project frontage, meaning the curb return shall start at the project property line, as shown in the image below. The existing curb cut at 90 Welsh shall be vacated as it is an inactive driveway. The sponsor shall work with the 90 Welsh property owner to restore a standard curb.



Follow-up for Bulbouts and Sidewalk Widening

- Pre-entitlement/Neck SDAT
- Sponsor to submit revised plans to SDAT showing required sidewalk widening
- Bulbout curb returns shall conform to SF Public Works' Standard Plan for Curb Bults. See: <https://sfpublicworks.org/sites/default/files/67%20CI75.pdf>
- Pre- or Post-entitlement
- Obtain relevant permits from BSM: www.sfpublicworks.org/services/permits

3. Utility Undergrounding

- Per San Francisco Subdivision Regulation, it is likely that the project is required to bury existing power lines underground along both Bryant and Welsh Street frontages.
- Be advised that the existing power lines along the Bryant Street frontage require a radial clearance of at least 10 feet, meaning new building facades cannot be within the 10' radial clearance of the existing power lines.

4. Off-Street Freight Loading

- SDAT reviewed submitted turn templates and confirmed the proposed curb cut width is appropriate.
- SDAT requires the project hire a flagger to guide trucks backing into the loading bays. (Required as part of the DLOPI).
- SDAT is concerned about internal circulation within the garage. In particular, trucks may take a while to get in and out of the loading bays due to limited space inside the garage, and such delay may slow the flow of traffic entering/exiting the garage.

中文詢問請電

Para información en Español llámanos al

Para su información en Tagalog tumawag sa

628.682.7550



SDAT REVIEW LETTER 2 PAGE 1-2

Housing Sustainability District Application Drawings

555-585 Bryant Street
San Francisco, CA 94107

05-27-2021

C - 098

Follow-up for curb cuts, off-street parking and loading	<ul style="list-style-type: none"> Pre-entitlement/Next SDAT submittal A written statement confirming a flagger will be hired.
Contacts	Coordinate with your assigned Planner

5. On-street Loading

- SDAT supports the proposed conceptual design for on-street loading zones, but the loading zone design requires more detailed review. The sponsor shall set up a joint meeting with Paul Karina (paul.karina@sfmta.com), SFMTA Color Curb Program Manager, and Karina Laiwet (karinalaiwet@sfdpw.org), Associate Engineer with the Public Works Disability Access Coordinator, to discuss the project's loading needs and corresponding accessibility requirements.

Follow-up	<ul style="list-style-type: none"> Pre-entitlement/Next SDAT sponsor to schedule a meeting with both SFMTA Loading Team & Public Works Accessibility Coordinator to coordinate design of loading zone(s).
Contacts	<ul style="list-style-type: none"> Post-entitlement (Post-Certificate of Occupancy) Sponsor to apply for on-street loading zones from the SFMTA permits from SFMTA. https://www.sfmta.com/online-color-curb-application Karina Laiwet (karinalaiwet@sfdpw.org), Associate Engineer with the Public Works Disability Access Coordinator Paul Karina (paul.karina@sfmta.com), SFMTA Color Curb Program Manager

6. Transformer

- The sponsor shall submit a written statement that confirms the proposed transformer location and design meets relevant SFPUC or PG&E standards.

Follow-up	<ul style="list-style-type: none"> Pre-entitlement/Next SDAT Coordinate with SFPUC or PG&E to ensure proposed transformer location meets relevant standards.
Contacts	<ul style="list-style-type: none"> Transformer Location (ROW v. Private Property): Coordinate with your assigned Current Planner on this item Transformer Location, Technical Feasibility: Coordinate with electrical power utility (SFPUC or PG&E) and Public Works BSM.

ADDITIONAL INFORMATION REQUIRED FOR NEXT SDAT REVIEW

- Existing/proposed curb cuts and curb cuts to be removed
- Street names
- Dimensions of existing and proposed sidewalk and curb extensions on plans
- Dimensions of existing and proposed curb cuts on plans
- Dimensions of existing and proposed transit stops
- Site plan with streetscape features (e.g., bulbouts, trees, transit shelters, benches, bike racks)
- Proposed street tree locations
- Adjacent ROW widths
- Locations of existing utility poles and hydrants
- Turn templates for _____
- Curb-to-curb section, including dimensions of tree wells and path of travel
- Proposed transformer room confirmation from SFPUC or PG&E
- A written statement clarifying that the Standard SDAT Comments document has been reviewed.

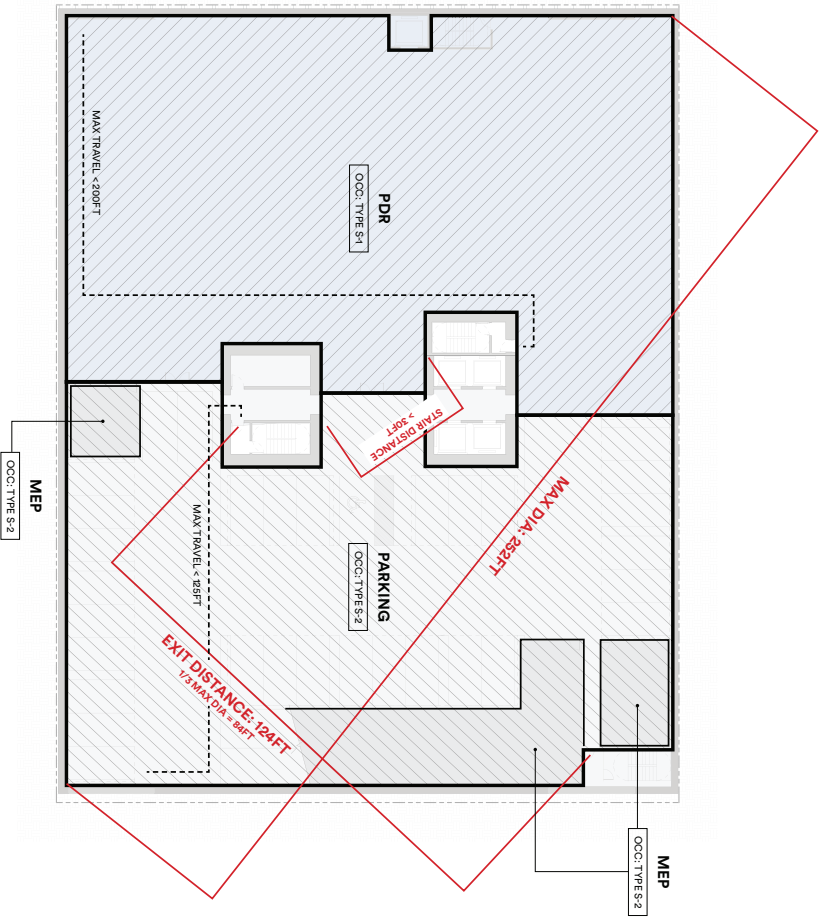
STANDARD SDAT COMMENTS

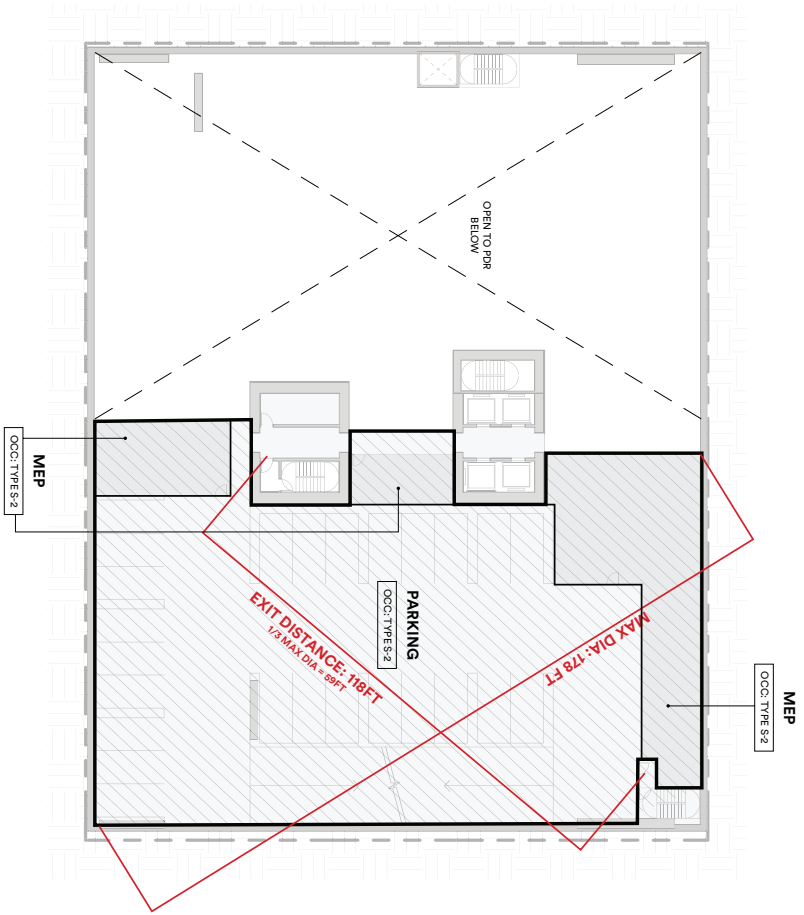
For your next SDAT submittal, please review the "Standard SDAT Comments" which can be found on the SDAT website (<https://sfplanning.org/project/street-design-advisory-team>), and include a written statement clarifying that this task has been completed and that all plans are consistent with guidelines/standards enumerated in the "Standard SDAT Comments".

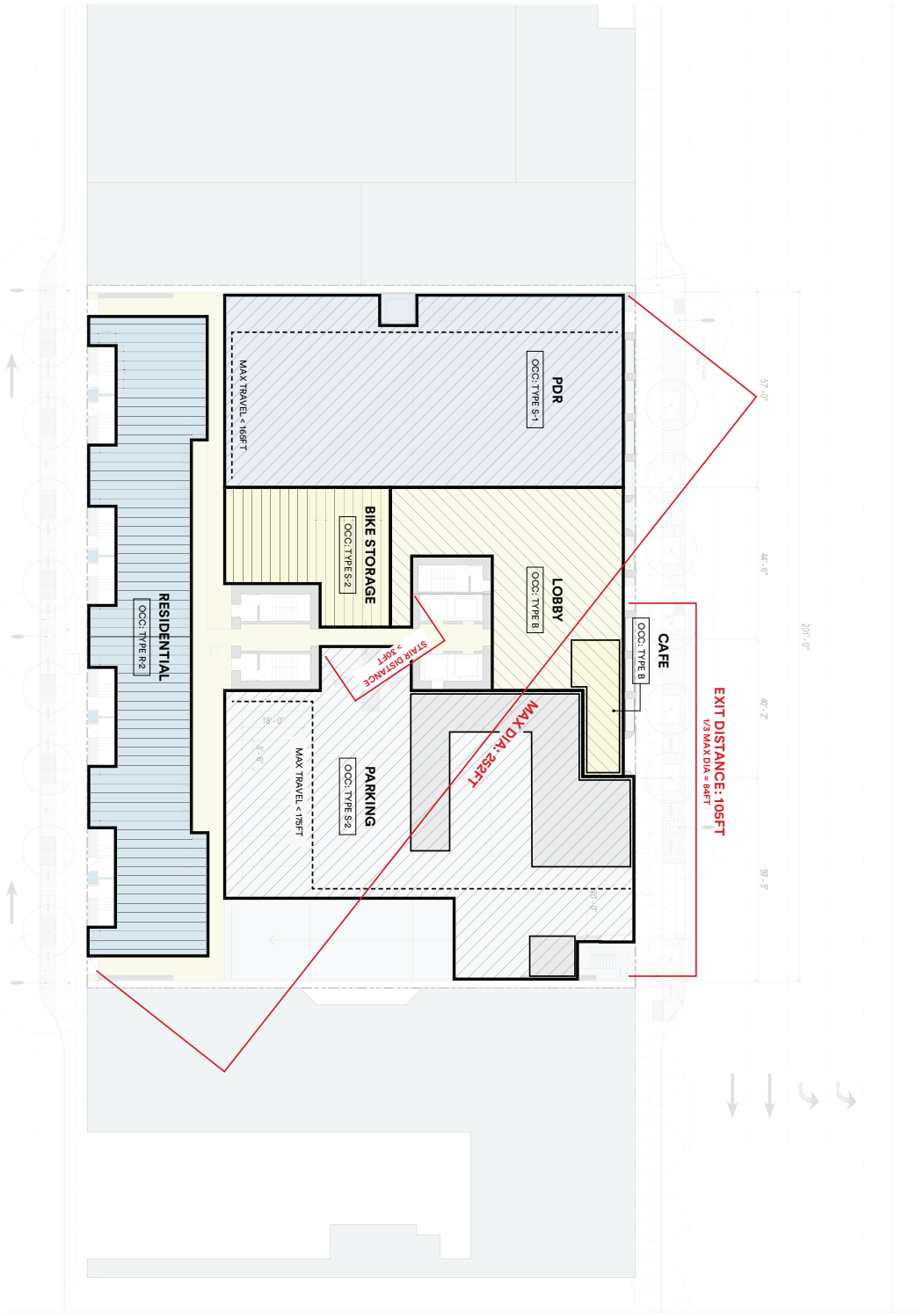
SDAT Members:

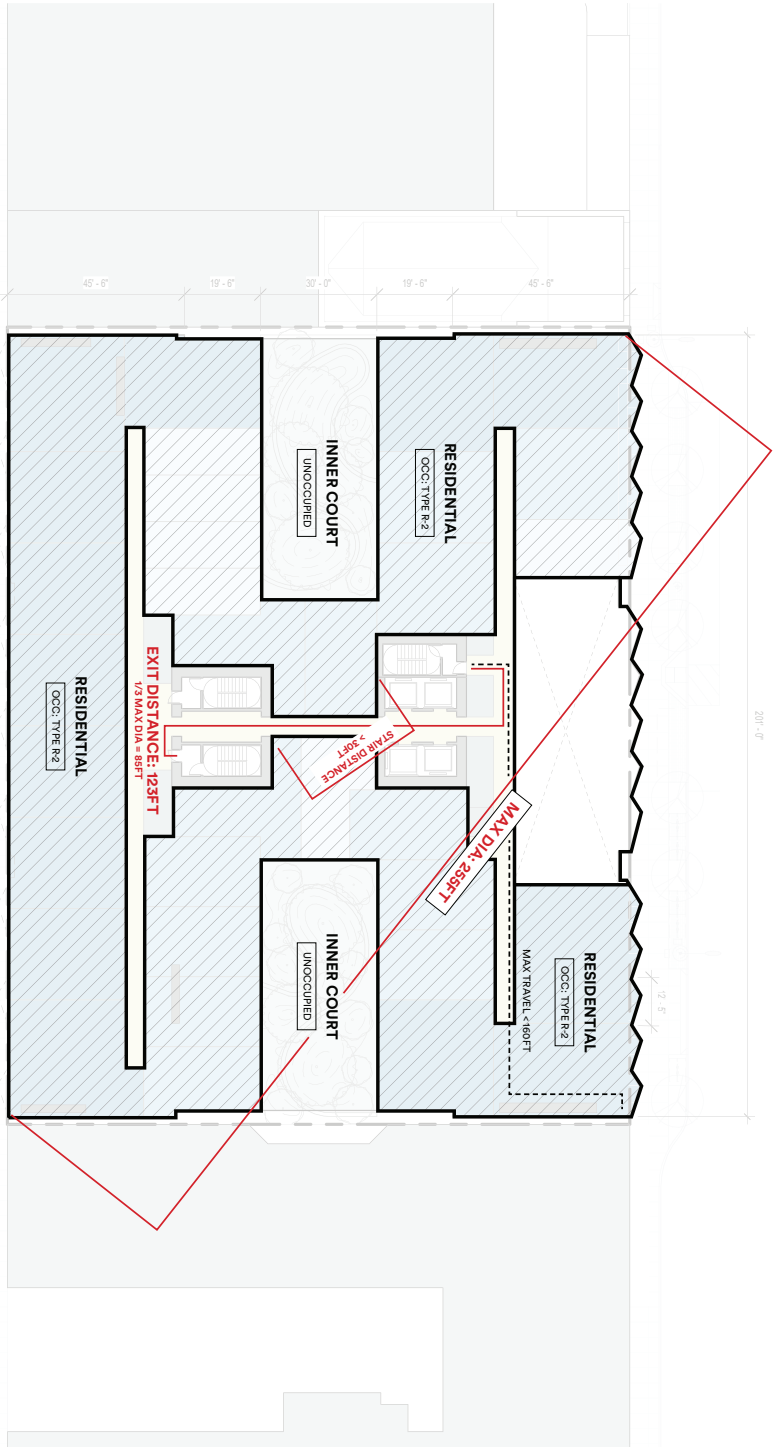
- SF Public Works:** Chris Buck, Bethane Gaime, Kevin Jensen, Jung Johnson, Karina Laiwet, Eric Lam, Debra Luske, Danny Phan, Suzanne Suskind, John Thomas, Michelle Woo, John Kwong, Jennifer Cooper
- SFMTA:** Paul Karina, Adrian Luang, Westley Myles, Francesca Napolitan, Ricardo Olea, Felipe Hobbs, Mike Sallaberry, Daniel Sheeter, Norman Wong, Dustin White
- SF Planning:** Ben Caldwell, Maria DeAlva, Kimberly Durandet, Nicholas Foster, Ryan Shum, Seung Yen Hong, Jessica Look, Ileana Salvadori, Maia Small
- SFPUC:** Derek Adams, Mira Chokshi, Hieu Doan, Molly Patrick, Joan Ryan, Sam Young
- SFPD:** Chad Law, Gerald Gauer

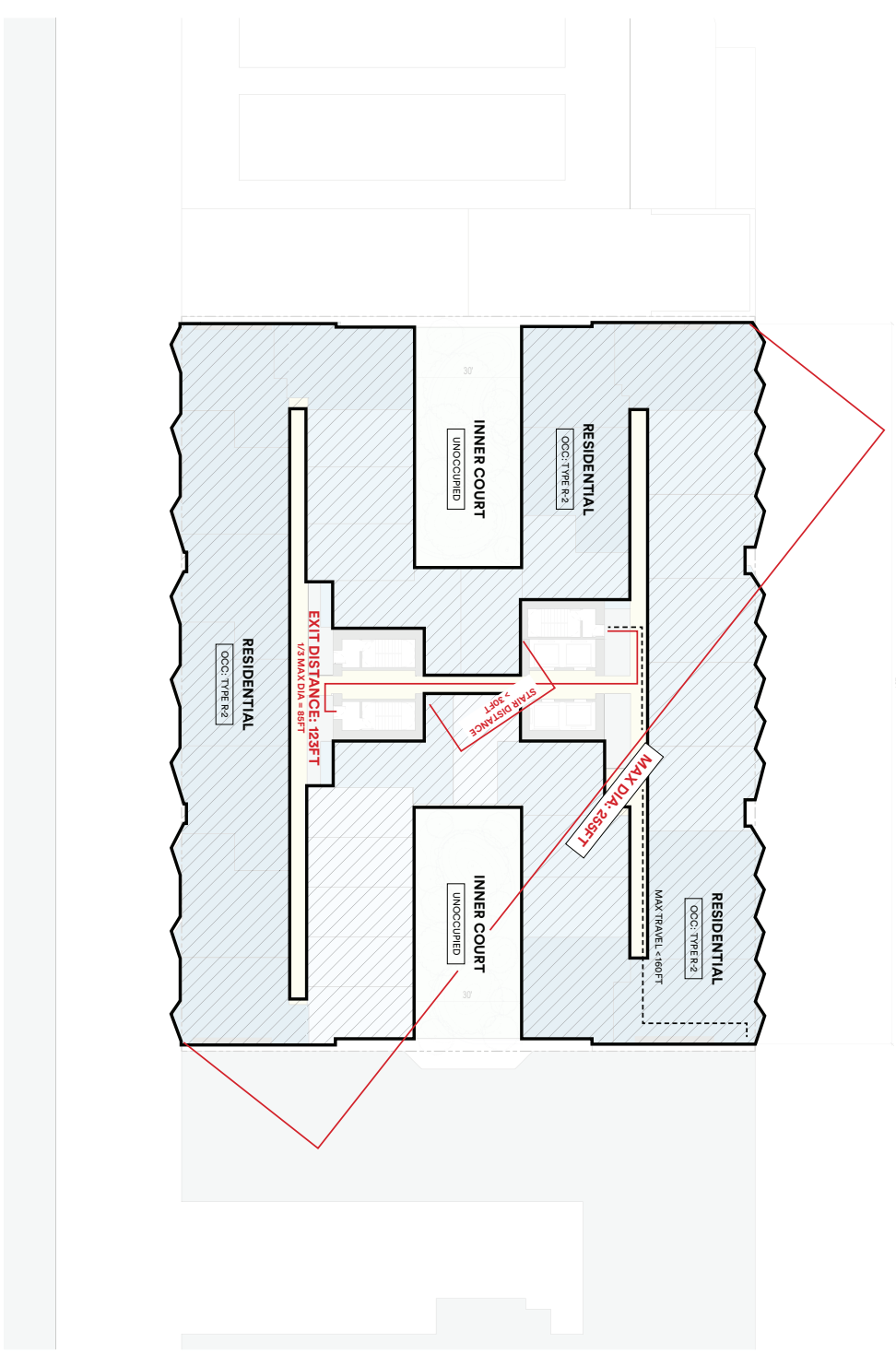
Egress Plans

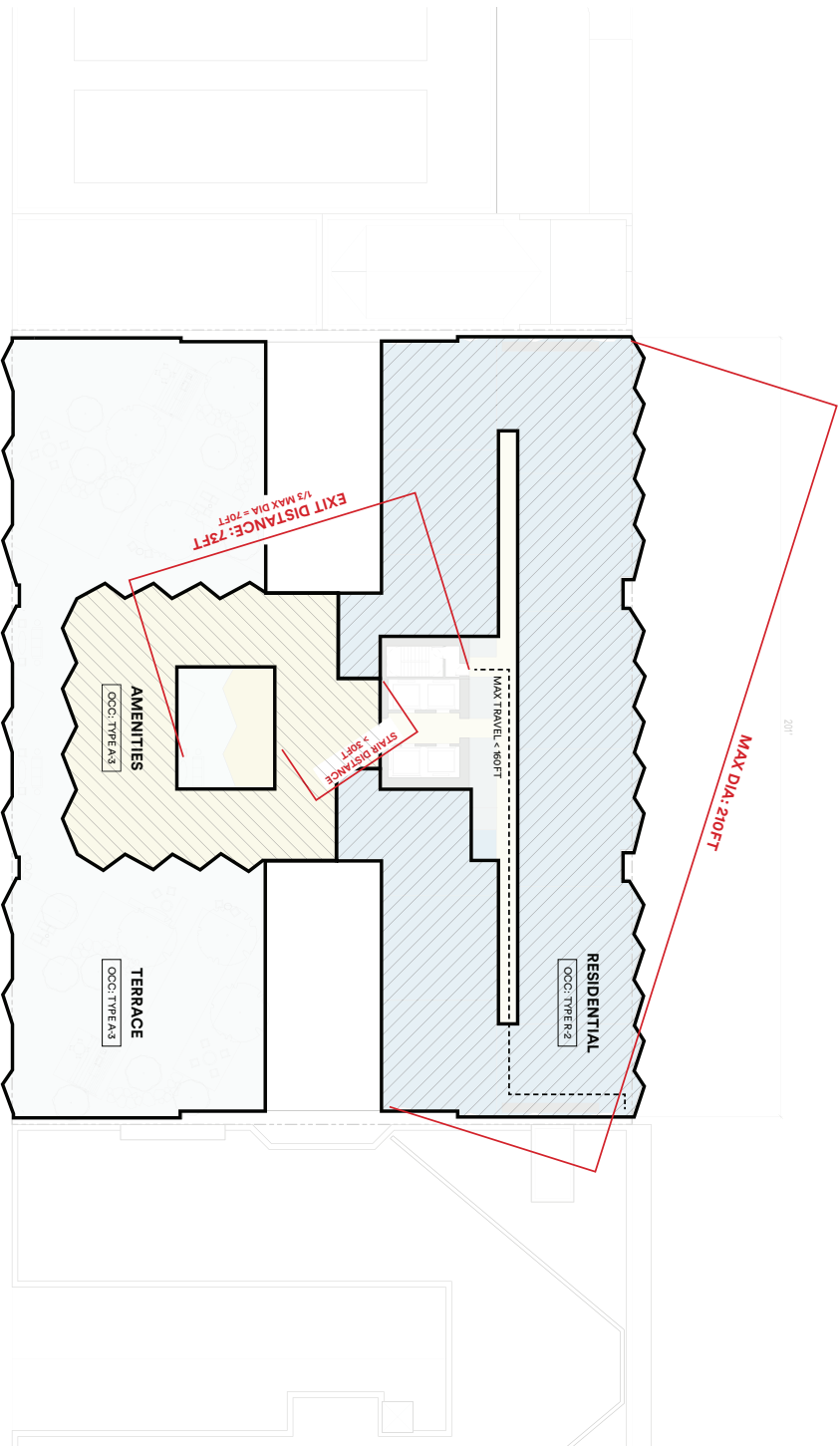


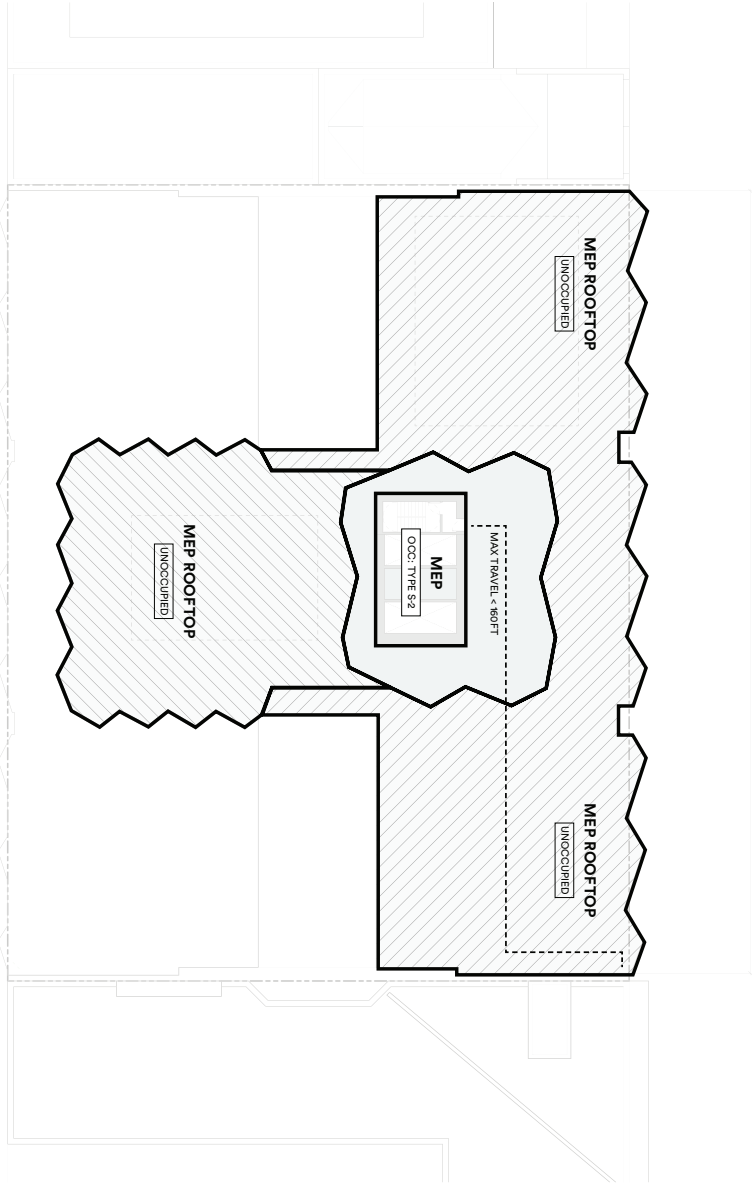












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**DEPARTMENT OF HOUSING AND COMMUNITY DEVELOPMENT
DIVISION OF HOUSING POLICY DEVELOPMENT**

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April 28, 2021

Rich Hills, Director
Planning Department
49 South Van Ness Avenue, Suite 1400
San Francisco, CA 94103

RE: City and County of San Francisco Housing Sustainability District Ordinance

Dear Rich Hills:

Thank you for submitting the City and County of San Francisco's (San Francisco) proposed ordinance establishing a housing sustainability district in Central South of Market (HSD- Central SOMA). This letter serves as the final determination by the California Department of Housing and Community Development (HCD) required pursuant to Government Code section 66202.

HCD has determined that the proposed HSD- Central SOMA ordinance addresses the requirements of housing sustainability districts, pursuant to Government Code sections 66200 through 66210. Please note that HCD has not conducted a full review of any design review standards for consistency with Government Code section 66207. In addition, please be aware that the Legislature has not appropriated funds for a zoning incentive payment, and as a result, San Francisco is not entitled to a zoning incentive payment pursuant to Government Code section 66202, subdivision (a)(2), or section 66204, subdivision (b), at this time. In the event the Legislature appropriates funds for zoning incentive payments, San Francisco should submit an application for a zoning incentive payment, including all of the information required by Government Code sections 66202, subdivisions (a) and (b), and 66204, subdivision (b).

HCD commends San Francisco for its leadership in advancing the state's housing goals. A housing and sustainability district is an important tool to increase housing supply and affordability while conserving existing housing stock affordable to lower-income households by streamlining and incentivizing housing production. HCD applauds San Francisco's long-standing commitment in promoting the development, conservation, and preservation of affordable housing.

Rich Hills, Director
Page 2

If HCD can provide any additional assistance, or if your staff have any questions, please contact Hillary Prasad, Housing Policy Specialist, at Hillary.Prasad@hcd.ca.gov.

Sincerely,

A handwritten signature in black ink that reads "Shannan West". The signature is written in a cursive, flowing style.

Shannan West
Land Use & Planning Unit Chief

cc: Kate Connor, Principal Planner (via email)